

APPENDIX D

CONSULTATION PROGRAM



CLASS EA FOR THE TEESWATER RIVER BRIDGE

MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT

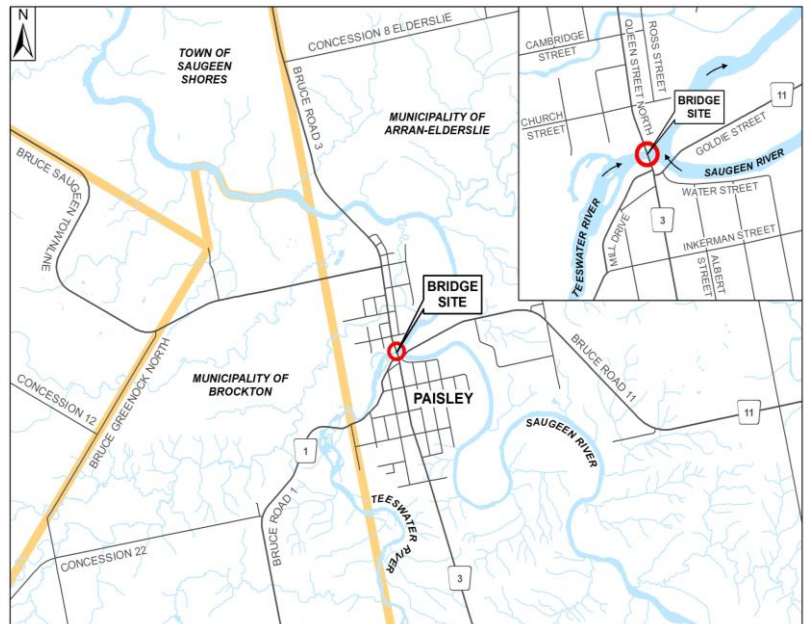
COUNTY OF BRUCE

NOTICE OF STUDY COMMENCEMENT

The County of Bruce has initiated a Class Environmental Assessment (Class EA) process to consider options associated with replacement of the Teeswater River Bridge which spans the Teeswater River along Bruce Road 3 in Paisley, immediately north of the intersection with Bruce Road 11 (as shown on the accompanying key plan). Recent inspections of the structure have identified significant deterioration with many bridge components. The County is considering alternatives associated with the new bridge design as well as detour alternatives to allow traffic to detour around the site during construction of the new crossing.

THE ENVIRONMENTAL ASSESSMENT PROCESS:

The planning for this project is following the planning process established for Schedule 'C' activities under the Municipal Class Environmental Assessment (Class EA) document. Schedule 'C' projects must complete all five phases of the Class EA, which is undertaken in order to identify potential environmental impacts associated with the proposal and to plan for appropriate mitigation of any impacts. The process includes consultation with the public, First Nation and Métis communities, project stakeholders and review agencies. This notice is being issued to advise of the start of study investigations. There will be additional opportunities for public input and involvement as the study progresses.



PUBLIC INVOLVEMENT:

Public input and comments are invited for incorporation into the planning and design of this project and will be received until November 29, 2019. Any comments collected in conjunction with the study, will be maintained on file for use during the project and may be included in project documentation. With the exception of personal information, all comments will become part of the public record. For further information regarding this project, please contact the project engineers: B.M. Ross and Associates: 62 North Street, Goderich, Ontario, N7A 2T4. Telephone (Toll Free): (888) 524-2641. Fax: (519) 524-4403. Kelly Vader, Environmental Planner (e-mail: kvader@bmross.net), within 30 days from the date of this Notice

This Notice Issued October 29, 2019

County of Bruce
Jim Donohoe, Engineering Manager

B. M. ROSS AND ASSOCIATES LIMITED
Engineers and Planners
62 North Street, Goderich, ON N7A 2T4
p. (519) 524-2641 • f. (519) 524-4403
www.bmross.net

File No. 13127

October 22, 2019

Review Agency
(see attached list)

**RE: Class EA to Replace the Teeswater River Bridge
County of Bruce (Paisley)**

The County of Bruce has initiated a Class Environmental Assessment (Class EA) process to consider options associated with replacement of the Teeswater River Bridge which spans the Teeswater River along Bruce Road 3 in Paisley, immediately north of the intersection with Bruce Road 11 (as shown on the accompanying key plan). Recent inspections of the structure have identified significant deterioration with many bridge components. The County is considering alternatives associated with the new bridge design as well as detour alternatives to allow traffic to detour around the site during construction of the new crossing.

The planning for this project is following the planning process established for Schedule "C" activities as described in the Municipal Engineers Association Municipal Class Environmental Assessment (Class EA) document. The purpose of the Environmental Assessment process is to identify any potential environmental impacts associated with the project and to plan for appropriate mitigation of any impacts. The process includes additional consultation with the public, First Nation and Métis communities, project stakeholders and government review agencies.

Your organization has been identified as possibly having an interest in this project and we are soliciting your input. Please forward your response to our office by November 29, 2019. If you have any questions or require further information, please contact the undersigned at kvader@bmross.net or by phone at 1-888-524-2641.

Yours very truly

B. M. ROSS AND ASSOCIATES LIMITED

Per _____
Kelly Vader, MCIP, RPP
Environmental Planner

KV:sd
Encl.
cc. Jim Donohoe, County of Bruce

**MUNICIPALITY OF ARRAN-ELDERSLIE
BRIDGE INFRASTRUCTURE MASTER PLAN**

REVIEW AGENCY CIRCULATION LIST

REVIEW AGENCY	
Ministry of the Environment, Conservation & Parks (London) - EA Coordinator	Mandatory Contact
Ministry of Natural Resources and Forestry Guelph	Potential Impact on Natural Features
Ministry of Tourism, Culture and Sport, Toronto	Potential Impact to Cultural Heritage Features
Ministry of Transportation (Owen Sound)	Potential Impact on Transportation Network
Municipality of Brockton	Adjacent Municipality
Municipality of Kincardine	Adjacent Municipality
Town of Saugeen Shores	Adjacent Municipality
Bruce County - Highways Department - Planning & Development Department	- Proponent - General Information
Saugeen Valley Conservation Authority	Potential Impact on Natural Features
Municipality of Arran-Elderslie	Adjacent Municipality
Department of Fisheries and Oceans Canada	Burlington
Bruce Grey Catholic District School Board Transportation Services	Impact on Transportation
Bluewater District School Board	Impact on Transportation

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Engineers and Planners
62 North Street, Goderich, ON N7A 2T4
p. (519) 524-2641 • f. (519) 524-4403
www.bmross.net

File No. 13127

October 22, 2019

Aboriginal Community
(see attached list)

**RE: Class EA to Replace the Teeswater River Bridge
County of Bruce (Paisley)**

The County of Bruce has initiated a Class Environmental Assessment (Class EA) process to consider options associated with replacement of the Teeswater River Bridge which spans the Teeswater River along Bruce Road 3 in Paisley, immediately north of the intersection with Bruce Road 11 (as shown on the accompanying key plan). Recent inspections of the structure have identified significant deterioration with many bridge components. The County is considering alternatives associated with the new bridge design as well as detour alternatives to allow traffic to detour around the site during construction of the new crossing.

The planning for this project is following the planning process established for Schedule "C" activities as described in the Municipal Engineers Association Municipal Class Environmental Assessment (Class EA) document. The purpose of the Environmental Assessment process is to identify any potential environmental impacts associated with the project and to plan for appropriate mitigation of any impacts. The process includes additional consultation with the public, First Nation and Métis communities, project stakeholders and government review agencies.

Your community has been identified as possibly having an interest in this project. For your convenience, a response form is enclosed. Please forward your response to our office by November 29, 2019. If you have any questions or require further information, please contact the undersigned at 519-524-2641 or by e-mail at kvader@bmross.net.

Yours very truly

B. M. ROSS AND ASSOCIATES LIMITED

Per _____
Kelly Vader, MCIP, RPP
Environmental Planner

KV:sd
Encl.

cc. Jim Donohoe, County of Bruce

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**COUNTY OF BRUCE
CLASS EA TO REPLACE PAISLEY BRIDGE
PROJECT: 13127**

ABORIGINAL CIRCULATION LIST

Chippewas of Nawash Unceded First Nation
Chief Gregory Nadjiwon
R.R. #5
Wiarton, ON N0H 2T0
executiveassistant@nawash.ca

Chippewas of Saugeen First Nation
Chief Lester Anoquot
Hwy. 21, R.R. # 1
Southampton, ON N0H 2L0
sfn@saugeen.org

Saugeen Ojibway Nation (SON) – Chippewas of Saugeen &
Chippewas of Nawash
Land Use Planning: Doran Ritchie
25 Maadookii Subdivision
Neyaashiinigmiing, ON N0H 2T0
d.ritchie@saugeenojibwaynation.ca

Historic Saugeen Métis
George Govier, Consultation Coordinator
204 High Street, Box 1492
Southampton, ON N0H 2L0

Métis Nation of Ontario
Suite 1100 – 66 Slater Street
Ottawa, ON K1P 5H1
consultations@metisnation.org

Great Lakes Métis Council
380 9th Street East
Owen Sound, ON N4K 1P3
greatlakesmetis@gmail.com

Response Form

Project Name: Paisley Bridge Class EA

Project Description: Class EA to Replace Bridge spanning the Teeswater River in downtown
Paisley

Project Location: Municipality of Arran-Elderslie, County of Bruce

(Key Plan of Project Location attached)

Please Detach and Return in Envelope Provided

Name of Aboriginal Community: _____

Please check appropriate box

☐

Please send additional information on this project

☐

We would like to meet with representatives of this project.

☐

We have no concerns with this project and do not wish to be consulted further

Project Name: Paisley Bridge EA **Location:** Arran-Elderslie **Proponent:** Bruce County

B. M. ROSS AND ASSOCIATES LIMITED
Engineers and Planners
62 North Street, Goderich, ON N7A 2T4
p. (519) 524-2641 • f. (519) 524-4403
www.bmross.net

File No. 13127

October 23, 2019

Dear Property Owner

**RE: Class EA to Replace the Teeswater River Bridge
County of Bruce (Paisley)**

The County of Bruce has initiated a Class Environmental Assessment (Class EA) process to consider options associated with replacement of the Teeswater River Bridge which spans the Teeswater River along Bruce Road 3 in Paisley, immediately north of the intersection with Bruce Road 11 (as shown on the accompanying key plan). Recent inspections of the structure have identified significant deterioration with many bridge components. The County is considering alternatives associated with the new bridge design as well as detour alternatives to allow traffic to detour around the site during construction of the new crossing.

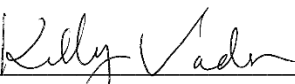
The planning for this project is following the planning process established for Schedule "C" activities as described in the Municipal Engineers Association Municipal Class Environmental Assessment (Class EA) document. The purpose of the Environmental Assessment process is to identify any potential environmental impacts associated with the project and to plan for appropriate mitigation of any impacts. The process includes additional consultation with the public, First Nation and Métis communities, project stakeholders and government review agencies.

As a property owner located in the vicinity of the bridge, you have been identified as possibly having an interest in the project and we are soliciting your input. Please forward any concerns or comments to our office by November 29th, 2019. Be advised that the study is just beginning. There will be additional opportunities for public input and comment as the study progresses.

If you have any questions or require further information, please contact the undersigned at 519-524-2641 or by e-mail at kvader@bmross.net.

Yours very truly

B. M. ROSS AND ASSOCIATES LIMITED

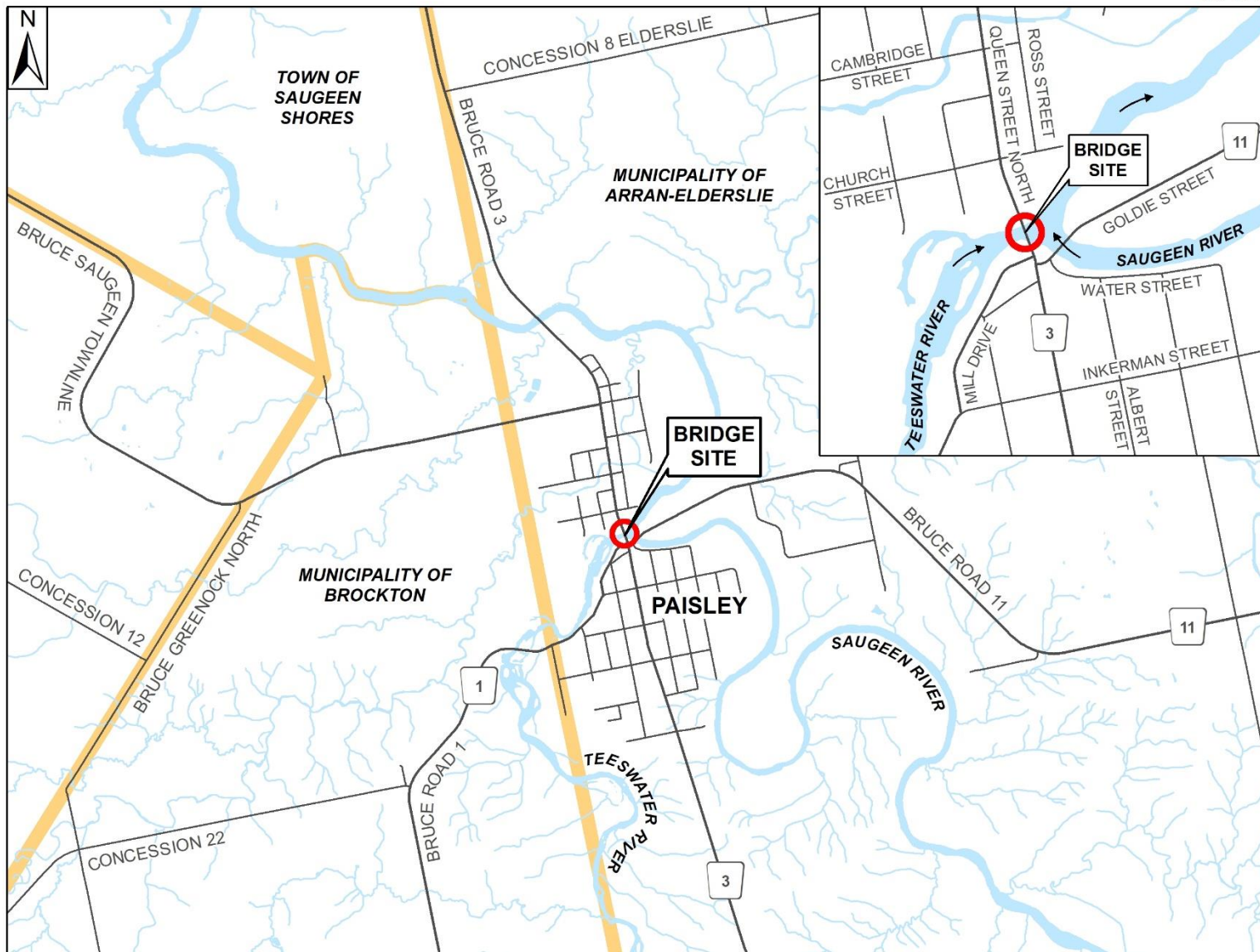
Per 
Kelly Vader, MCIP, RPP
Environmental Planner

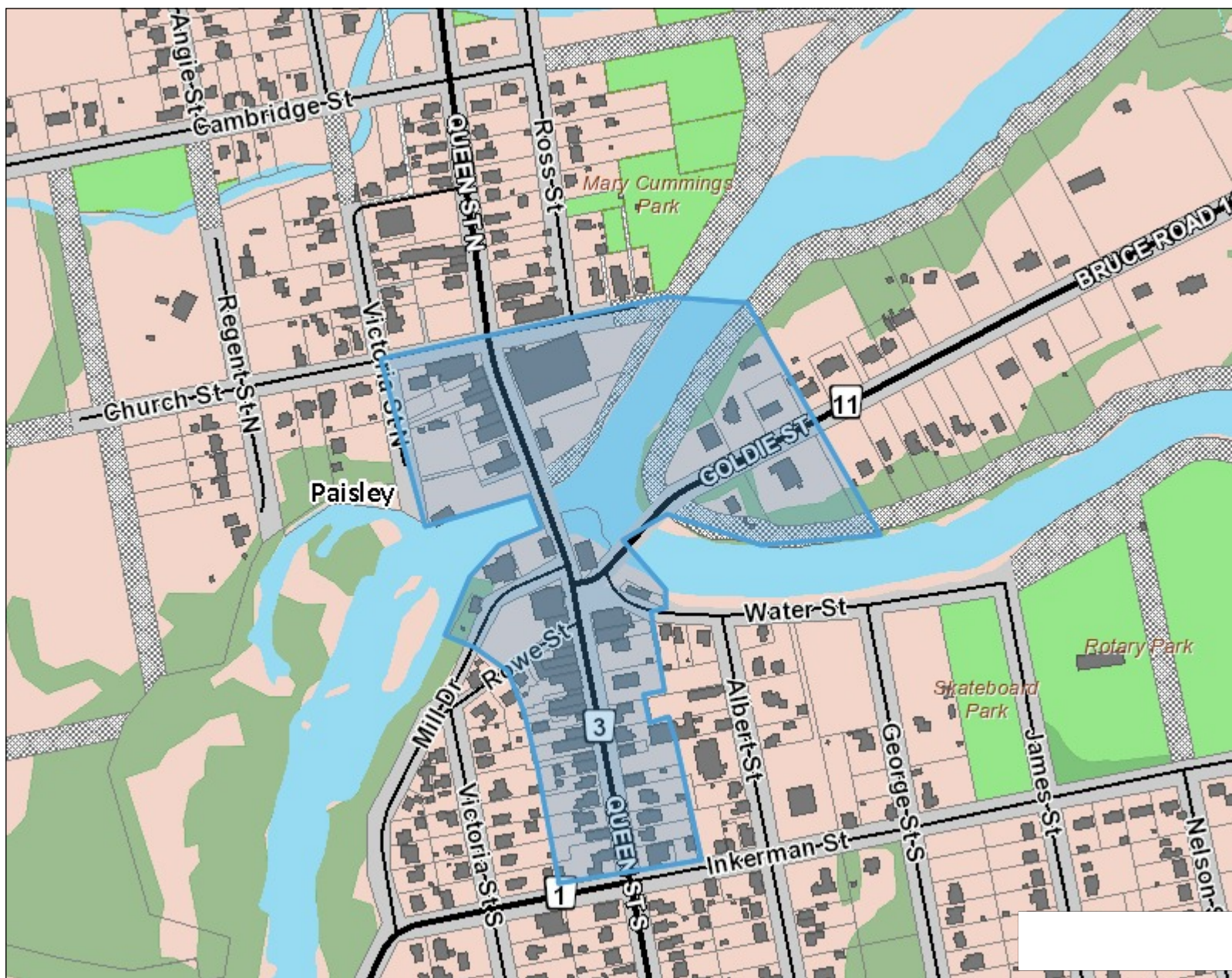
KV:sd

Encl.

cc. Jim Donohoe, County of Bruce

Z:\13127-Bruce_County-EA-Paisley_Bridges\Projects\Class EA\Public Consultation\13127-19Oct23-Property Owner Let.docx





Legend

- Ferry
- Provincial Highway
- County Road
- Municipal or Other Road
- OBM Building Location
- OBM Building Footprint
- Building Footprint
- Road Allowance/Right-of-way
 - Private Road Allowance
 - Right-of-Way
 - Road Allowance or Condo Road
 - Unopened Road Allowance
- Body of Water
- Evaluated Wetland
- Watercourse
 - Permanent Stream
 - Intermittent Stream
- Provincial Park
- Fathom Five
- National Park
- County Forest Boundary
- County Forest
- Wooded Area
- Conservation Authority Property
- Parkland
- Golf Course
- Body of Water (small scale)
- Adjacent Counties (small scale)
- Wetland
- Adjacent Counties
- Adjacent Counties
- Lake Huron and Georgian Bay
- Adjacent Counties

Notes

0.3 0 0.13 0.3 Kilometers

NAD_1983_UTM_Zone_17N
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This map is a user generated static output from an Internet mapping site and is for reference only. Data layers that appear on this map may or may not be accurate, current, or otherwise reliable.

THIS MAP IS NOT TO BE USED FOR NAVIGATION

Kelly Vader

From: Chris Hachey <hsmasstlrcc@bmts.com>
Sent: Tuesday, October 29, 2019 11:33 AM
To: Kelly Vader
Subject: Request for Comments - Bruce County (Paisley) - Class EA to Replace Teeswater River Bridge

Your File: 13127
Our File: Bruce County - Arran-Elderslie (Projects)

Ms. Vader,

The Historic Saugeen Métis (HSM) Lands, Resources and Consultation Department appreciates the opportunity to be consulted regarding the County of Bruce, Class EA to Replace the Teeswater River Bridge in Paisley. HSM interests related to the study largely focus on environmental effects / sustainability and the potential for archaeological resources associated with future development.

HSM looks forward to further consultation regarding this project as information becomes available.

Regards,

Chris Hachey

Assistant Coordinator, Lands, Resources and Consultation

Historic Saugeen Métis
204 High Street
Southampton, Ontario, N0H 2L0
Telephone: (519) 483-4000
Fax: (519) 483-4002
Email: hsmasstlrcc@bmts.com

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Paisley freshmart

Your Neighbourhood Grocer

P.O. Box 340 436 Queen Street North Paisley, ON N0G 2N0

www.paisleyfreshmart.ca

519-353-3700

November 1, 2019

B.M. Ross and Associates
62 North Street
Goderich, ON N7A 2T4

Re: Class EA for the Teeswater River Bridge Replacement

Dear Kelly Vader;

As residents and business owners of Paisley, we are thrilled that planning is underway to consider options for the replacement of the Teeswater River Bridge on Bruce County Road 3 in Paisley. There is no doubt that it is well worn due to very high traffic and heavy spring flows.

As owners of the only full service grocery store in Paisley, on the north side of the bridge, we have concerns. We also listen to our customers express anxieties. It is our hope that you will consider our apprehensions.

1. We rely heavily on summer tourists travelling to campgrounds and cottages from early June until early September, stopping at Paisley freshmart for supplies and food. This income sustains us through the quieter months of the year to ensure we remain a viable independent, family-owned business, supporting 15 staff and their families.
 - Is it possible to have full closure of the bridge and highway either in the spring or in the fall, and the rest of the time, have the bridge partially open?
 - How long will we be without a bridge and can this be as short as possible?
2. As long-time residents of Paisley, we realize that a current road detour would be at least 8 miles around the entire village, either side. We have concerns about Paisley residents on the south side of the bridge, not having access to groceries.
 - Is it possible to put in a temporary foot bridge across the Teeswater or Saugeen River?
 - Is it possible to create or open a closer roadway for the detour that would make it much shorter?

We are truly concerned that without summer tourist traffic and without local residents being able to access the store easily, our business will be devastated and not be able to sustain the loss. We plan to offer grocery delivery service and any other services we can provide.

We plead with you, to please keep the road & bridge closure as short as possible.

We would request no more than two months (8 weeks).

Thank you for considering our comments.

Robert and Gail Fullerton, Residents and Owners



1078 Bruce Road 12, P.O. Box 150, Formosa ON Canada N0G 1W0
Tel 519-367-3040, Fax 519-367-3041, publicinfo@svca.on.ca, www.svca.on.ca

SENT ELECTRONICALLY ONLY (kvader@bmross.net)

November 19, 2019

B. M. Ross and Associates Limited
62 North Street
Goderich, ON
N7A 2T4

ATTENTION: Kelly Vader, RPP, MCIP
Environmental Planner

Dear Mrs. Vader:

RE: Notice of Initiation of Class Environmental Assessment, Replacement of the Teeswater River Bridge
County of Bruce
Geographic Village of Paisley
Township of Arran-Elderslie

The Saugeen Valley Conservation Authority (SVCA) has received correspondence regarding the initiation of a Class Environmental Assessment (Class EA) to consider options associated with the replacement of the Teeswater River Bridge in Paisley. The SVCA would be interested in any works that may be proposed in or adjacent to the Teeswater River and the Saugeen River. Our initial comments are as follows:

1. Works in or adjacent to the Teeswater River and the Saugeen River are subject to the SVCA's Development, Interference with Wetlands and Alterations to Shorelines and Watercourses Regulation, Ontario Regulation 169/06, as amended, made under the *Conservation Authorities Act*. Consequently, any works in or adjacent to the watercourses will require our review and permit.
2. The SVCA would be concerned with works that may be constructed, temporarily or permanently, that may impact areas adjacent to the watercourses, the watercourses themselves and/or flows in them. Accordingly, details should be provided so that SVCA staff may review the works, and any associated remedial measures, to comment appropriately.
3. The SVCA would be concerned with any changes that may increase flow velocities or negatively impact flow regimes and the passage of ice floes which in turn may impact the possibility of flooding and/or erosion on upstream and downstream lands.
4. The SVCA has floodplain mapping for the Village of Paisley. SVCA staff will require the design of the structure to address floodplain issues at this location of the Teeswater River and affected floodplain areas.



Watershed Member Municipalities

Municipality of Arran-Elderslie, Municipality of Brockton, Township of Chatsworth, Municipality of Grey Highlands, Town of Hanover, Township of Howick, Municipality of Morris-Turnberry, Municipality of South Bruce, Township of Huron-Kinloss, Municipality of Kincardine, Town of Minto, Township of Wellington North, Town of Saugeen Shores, Township of Southgate, Municipality of West Grey

5. Works in or adjacent to the watercourses such as those contemplated in the Environmental Assessment should be scheduled such that they could be undertaken during periods of low flows and at a time of year when elevated flow levels and velocities would not be expected.
6. Please note that the SVCA owns and maintains the flood control dyke adjacent to the Teeswater River Bridge. Additionally, the north end of the bridge is part of the Paisley Flood Control Project. Any alterations to the north end of the bridge or the dyke system will require our review and permit.
7. In the past, Conservation Authorities served as the first point of contact and the local service provider for review of Section 35 of the previous version of the Fisheries Act, and had entered into agreements with Fisheries and Oceans Canada to facilitate this process. Changes to the Fisheries Act effective November 25, 2013, have resulted in the cancellation of these agreements. It is now the responsibility of the proponent to contact the Department of Fisheries and Oceans at 1-855-852-8320 or <http://www.dfo-mpo.gc.ca/pnw-ppe/index-eng.html> to ensure their project addresses the Fisheries Act.

We trust this information will be of assistance to you. Thank you for the opportunity to provide our initial comments. The SVCA appreciates the opportunity to review this project as it develops and accordingly requests that you continue to notify our Authority at appropriate stages of design and review. If you have any questions, do not hesitate to contact our office.

Sincerely,



Paul Elston, P. Eng
Regulations Officer
Saugeen Conservation

PE/pe

cc: Jo-Anne Harbinson, Manager, Water Resources and Stewardship Services, SVCA, (via email)
Jim Donohoe, County of Bruce (via email)
Mark Davis, Authority Member, SVCA (via email)

Kelly Vader

From: Jack Van Dorp <JVanDorp@brucecounty.on.ca>
Sent: Tuesday, November 19, 2019 4:06 PM
To: kvader@bmross.net
Cc: Mark Paoli
Subject: Class EA to replace the Teeswater River Bridge
Attachments: image001.png; BRU_Logo_CMYK_Pos_7fe4ac59-8ed1-4449-be48-9e77bf2cdb4c.jpeg

Dear Ms. Vader,

Thank you for the opportunity to provide input comments to this project which is being undertaken by our partner department Bruce County Transportation and Environmental Services.

- The Planning Department supports the anticipated investment in this key piece of infrastructure for the County and the Community of Paisley.
- We encourage consideration of bridge detail design that is supportive of the Paisley Community identity as the “Artistic River Village” and provides for a high-quality pedestrian experience in this downtown location while also addressing the vehicular transportation function.
- We recommend consultation with Saugeen Valley Conservation Authority regarding historical and predicted floodwater flows for the Teeswater River and a design that provides a robust safety factor
- We encourage coordination with the anticipated redevelopment of the Paisley Inn property, located at 604 Queen Street South in close proximity to the bridge site, as there may be opportunities to minimize disruption to traffic flows and ensure adequate stormwater flow capacity. (development site shown in red on attached airphoto)
- This Bridge replacement may present an opportunity to work with the Municipality of Arran Elderslie to develop a pedestrian / bicycle route using the railway trestle bridge and municipal properties between the rail trail and Church Street (conceptual route in blue on attached airphoto generally follows existing informal trail)



Jack Van Dorp
Senior Planner
Planning and Development
Corporation of the County of Bruce

519-534-2092
www.brucecounty.on.ca



Individuals who submit letters and other information to Council and its Committees should be aware that any personal information contained within their communications may become part of the public record and may be made available through the agenda process which includes publication on the County's website.

If you have received this communication in error, please notify the sender immediately and delete all copies (electronic or otherwise). Thank you for your cooperation.

If you feel that this email was commercial in nature and you do not wish to receive further electronic messages from the County of Bruce, please click on the following link to unsubscribe:

<http://machform.brucecounty.on.ca/view.php?id=22357>. Please be advised that this may restrict our ability to send messages to you in the future.

**Ministry of Heritage, Sport,
Tourism, and Culture Industries**

Programs and Services Branch
401 Bay Street, Suite 1700
Toronto, ON M7A 0A7
Tel: 416.314.7182

**Ministère des Industries du Patrimoine,
du Sport, du Tourisme et de la Culture**

Direction des programmes et des services
401, rue Bay, Bureau 1700
Toronto, ON M7A 0A7
Tél: 416.314.7182



13 December 2019

EMAIL ONLY

Kelly Vader, Environmental Planner
B. M. Ross and Associates Limited
62 North Street
Goderich, ON N7A 2T4
kvader@bmross.net

MHSTCI File : 0011648
Your File : 13127
Proponent : County of Bruce
Subject : Notice of Commencement – Schedule C – Municipal Class EA
Project : Replacement of the Teeswater River Bridge
Location : Bruce Road 3 in the community of Paisley, Township of Arran-Elderslie, County of Bruce.

Dear Kelly Vader:

Thank you for providing the Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI) with the Notice of Commencement for the above-referenced project. MHSTCI's interest in this environmental assessment (EA) project relates to its mandate of conserving Ontario's cultural heritage, which includes:

- Archaeological resources (including land and marine);
- Built heritage resources (including bridges and monuments); and,
- Cultural heritage landscapes.

Project Summary

The County of Bruce has initiated a Class EA process to consider options associated with the replacement of the Teeswater River Bridge where Bruce Road 3 (Queen Street South) spans the Teeswater River. Recent inspections of the structure have identified deterioration and the County is considering alternatives associated with the new bridge design and detour alternatives during construction of the new crossing.

Identifying Cultural Heritage Resources

While some cultural heritage resources may have already been formally identified, others may be identified through screening and evaluation. Indigenous communities may have knowledge that can contribute to the identification of cultural heritage resources, and we suggest that any engagement with Indigenous communities includes a discussion about known or potential cultural heritage resources that are of value to these communities. Municipal Heritage Committees, historical societies and other local heritage organizations may also have knowledge that contributes to the identification of cultural heritage resources.

Municipal Heritage Bridges: Cultural, Heritage & Archaeological Resources Assessment Checklist

Under the EA process, the proponent is required to determine a project's potential impact on cultural heritage resources. The Municipal Engineers Association provides screening criteria for work on bridges that falls under the Municipal Class EA with a [checklist](#) and [background material](#) available online, developed in coordination with MHSTCI.

Part A – Municipal Class EA Activity Selection

Please use the [checklist](#) and [background material](#) to determine the Municipal Class EA schedule (A, A+, B or C) for the project. Completing the remainder of this checklist determines what technical cultural heritage studies may be required. The project has been identified as a Schedule C undertaking.

Part B - Cultural Heritage Assessment

If Part B of the checklist determines that the bridge or study area warrants the preparation of a Cultural Heritage Evaluation Report (CHER), and the undertaking of a Heritage Impact Assessment (HIA), our ministry's [Info Sheet #5: Heritage Impact Assessments and Conservation Plans](#) outlines the scope of HIAs. CHERs and HIAs are to be prepared by qualified consultants. Please send HIAs to MHSTCI for review and make copies available to local organizations or individuals who have expressed an interest in cultural heritage.

Part C – Heritage Assessment

If Part C of the checklist determines that the CHER has identified heritage features on the project and recommends that a Heritage Impact Assessment (HIA) be undertaken, our Ministry's [Info Sheet #5: Heritage Impact Assessments and Conservation Plans](#) outlines the scope of HIAs. CHERs and HIAs are to be prepared by qualified consultants. Please send HIAs to MHSTCI for review and make copies available to local organizations or individuals who have expressed an interest in cultural heritage.

Part D – Archaeological Resources Assessment

If Part D of the checklist establishes that an archaeological assessment is required, it is to be conducted by an archaeologist licenced under the *Ontario Heritage Act (OHA)*, who is responsible for submitting the report directly to MHSTCI for review. MHSTCI archaeological sites data are available at archaeology@ontario.ca.

After completing the checklist, please update MHSTCI on the project Class EA schedule and whether any technical cultural heritage studies will be completed for the project. Please provide all technical heritage studies to MHSTCI before issuing a Notice of Completion or commencing any of work on site.

Environmental Assessment Reporting

All technical cultural heritage studies and their recommendations are to be addressed and incorporated into EA projects. If the screening has identified no known or potential cultural heritage resources, or no impacts to these resources, please include the completed checklists and supporting documentation in the EA report or file.

Thank you for consulting MHSTCI on this project. Please continue to do so through the EA process, and contact me for any questions or clarification.

Sincerely,

Neil MacKay
Heritage Planner
Heritage Planning Unit
neil.mackay@onatrio.ca

Copied to: Jim Donohoe, County of Bruce

It is the sole responsibility of proponents to ensure that any information and documentation submitted as part of their EA report or file is accurate. MHSTCI makes no representation or warranty as to the completeness, accuracy or quality of the any checklists, reports or supporting documentation submitted as part of the EA process, and in no way shall MHSTCI be liable for any harm, damages, costs, expenses, losses, claims or actions that may result if any checklists, reports or supporting documents are discovered to be inaccurate, incomplete, misleading or fraudulent.

Please notify MHSTCI if archaeological resources are impacted by EA project work. All activities impacting archaeological resources must cease immediately, and a licensed archaeologist is required to carry out an archaeological assessment in accordance with the Ontario Heritage Act and the Standards and Guidelines for Consultant Archaeologists.

If human remains are encountered, all activities must cease immediately and the local police as well as the Registrar, Burials of the Ministry of Government and Consumer Services (416-326-8800) must be contacted. In situations where human remains are associated with archaeological resources, MHSTCI should also be notified to ensure that the site is not subject to unlicensed alterations which would be a contravention of the *Ontario Heritage Act*.



1078 Bruce Road 12, P.O. Box 150, Formosa ON Canada N0G 1W0
Tel 519-367-3040, Fax 519-367-3041, publicinfo@svca.on.ca, www.svca.on.ca

SENT ELECTRONICALLY ONLY (aross@bmross.net)

January 16, 2020

B. M. Ross and Associates Limited
62 North Street
Goderich, ON
N7A 2T4

ATTENTION: Andrew Ross, P.Eng.

Dear Mr. Ross:

RE: Class Environmental Assessment, Replacement of the Teeswater River Bridge
County of Bruce
Geographic Village of Paisley
Municipality of Arran-Elderslie

Saugeen Valley Conservation Authority (SVCA) staff have received your letter dated November 25, 2019 regarding the above noted project. The letter described a few preliminary ideas regarding replacement of the Teeswater River Bridge as well as requested information regarding Paisley floodplain mapping, stream gauge data and frequencies, design and/or as-recorded drawings for the Paisley dyke system, and applicable photo records of flood height and ice conditions.

Paisley floodplain mapping, stream gauge data, drawings of the Paisley dyke system and photo records have already been provided to you via email.

Proposed Development

As detailed in your letter, the Teeswater River Bridge will be replaced within the Queen Street road allowance, in the same location as the existing bridge. The following details regarding the bridge replacement have been provided:

- The bridge is expected to carry two lanes of traffic, with sidewalks on both sides;
- The bridge span and pier/abutment arrangement may change from existing;
- The south abutment may be relocated based on the function of the existing millrace;
- Traffic from Queen Street and County Road 3 will need to be diverted from the site for approximately 1 year.

The Regulation

The proposed replacement of the Teeswater River Bridge is subject to the SVCA's Development, Interference



Watershed Member Municipalities

Municipality of Arran-Elderslie, Municipality of Brockton, Township of Chatsworth, Municipality of Grey Highlands, Town of Hanover, Township of Howick, Municipality of Morris-Turnberry, Municipality of South Bruce, Township of Huron-Kinloss, Municipality of Kincardine, Town of Minto, Township of Wellington North, Town of Saugeen Shores, Township of Southgate, Municipality of West Grey

with Wetlands and Alterations to Shorelines and Watercourses Regulation (Ontario Regulation 169/06, as amended). This Regulation is in accordance with Section 28 of the Conservation Authorities Act, R.S.O. 1990, Chap. C. 27, and requires that a person obtain the written permission of the SVCA prior to any “development” in a Regulated Area or alteration to a wetland or watercourse.

“Development”

Subsection 28(25) of the Conservation Authorities Act defines “development” as:

- a) the construction, reconstruction, erection or placing of a building or structure of any kind;*
- b) any change to a building or structure that would have the effect of altering the use or potential use of the building or structure, increasing the size of the building or structure or increasing the number of dwelling units in the building or structure;*
- c) site grading; or,*
- d) the temporary or permanent placing, dumping or removal of any material, originating on the site or elsewhere.*

“Alteration”

According to Section 5 of Ontario Regulation 169/06, as amended, alteration generally includes the straightening, diverting, or interference in any way with the existing channel of a river, creek, stream, or watercourse, or the changing or interfering in any way with a wetland.

SVCA Regulations Comments:

SVCA Regulations staff have reviewed your November 25, 2019 letter and Detour Option 3 and offer the following comments for your consideration:

1. Temporary Detour Route – Option 3

It is the understanding of SVCA staff that several detour options will be considered on municipal roads and County highways as part of the Class EA process. A drawing of Detour Option 3, dated November 11, 2019, was provided with your letter. Detour Option 3 proposes placement of a temporary modular bridge across the Saugeen River, connecting Goldie Street to Church Street. It was noted in your letter that the approach for Detour Option 3 would penetrate the top portion of the existing flood control dyke on the north side of the Saugeen River. It should be noted that the SVCA owns and maintains the flood control dyke system. At this time, it is unclear to SVCA staff how removal of the top portion of the dyke would impact the risk of flooding in the Village of Paisley. It would need to be demonstrated to SVCA staff that interference with the flood control dyke would not lead to any short- or long-term impacts on the control of flooding.

Please provide details on any additional detour routes that have been considered thus far.

2. Potential Placement of Fill in the Floodplain

Based on our review of Detour Option 3, you have proposed a temporary modular bridge placed between Goldie Street and Church Street. It is in the opinion of SVCA staff that the placement of the temporary modular bridge would involve placement of fill in the floodway and the flood fringe. SVCA staff would require that a

cut/fill analysis of the site be performed as well as provide comment on the potential impacts of flooding from the placement of fill in the floodway. In accordance with Policy 4.7.1-2, it will need to be demonstrated to SVCA staff that the placement of fill will not negatively affect the control of flooding.

Cut/fill analysis and comment on potential impacts to the control of flooding would also be required for the replacement Teeswater River Bridge.

3. *Changes in Upstream and Downstream Flows*

SVCA staff would be concerned with any changes in upstream or downstream flood depths and velocities as a result of both the temporary modular bridge and the replacement Teeswater River Bridge. Additionally, staff would be concerned with any changes in flow regimes resulting from placement of either structure. Site specific studies will be required to examine the potential impact on the floodplain for the 5, 25, 50, 100 year and the Hurricane Hazel Flood Event Standard. There should be minimal increase in the backwater elevation produced upstream from all proposed works when compared to existing conditions.

It should be noted that the floodplain mapping currently used for Paisley is dated, SVCA staff may need updated floodplain mapping of the area if the impact of flooding is expected to change with the proposed works.

4. *Presence of Ice Floes and Ice Jamming*

The Teeswater and Saugeen Rivers in Paisley are particularly susceptible to ice floes and ice jamming. The extent of ice floes and ice jamming in Paisley can be unpredictable in nature and may damage the floodway itself and/or any structures residing in the floodway. Ice floes and ice jamming occurring in the Teeswater River, the Saugeen River, or at the intersection of the two Rivers will vary in frequency and depth during spring runoff events.

Ice jamming in Paisley could result in potential blockages of both the temporary modular bridge and the replacement Teeswater River Bridge. It would need to be demonstrated to SVCA staff that the presence of the temporary modular bridge, as detailed in Detour Option 3, or replacement new Teeswater River Bridge would not impact the control of flooding under ice jamming conditions. Physical blockage of either bridge is also possible depending on the severity of the ice floes and ice jamming experienced.

SVCA Flood Control Dyke Comments:

The SVCA in partnership with the Province of Ontario and the Municipality constructed the dyke system in Paisley to protect life and property during a major runoff event.

This dyke system includes a portion of flood control dyke upstream of the Queen Street bridge as well as a length of dyke downstream of the bridge. The bridge itself forms a part of the flood control plan using the cement panels located in the northern section of the railing on both sides of the bridge.

In addition, stormwater outfalls are located upstream and immediately downstream of the bridge and form an integral part in the management of stormwater on the lands side of the dyke, and project urban areas from back flow river water reaching the urban areas.

Reports and plans have been provided to you on the construction of the dykes as well as other floodplain information.

As the SVCA staff have strong interest in the protection of the residents and infrastructure for the Village of Paisley, we are concerned with the works that could affect the operations of the dykes as intended both during construction and post construction. This will need to be addressed both during the bridge work over the Teeswater River as well as for the proposed temporary crossing/bridge over the Main Saugeen River. Based on the information known to date, SVCA staff will require confirmation from B. M. Ross on the following, additional comments/concerns may be brought forward as more information is provided to the SVCA on this project:

- a) That any changes, work, activities that will impact the flood control dyke are to be identified.
- b) Where changes, work or activities are proposed on the dyke, information needs to be provided to the satisfaction of the SVCA as to how the dyke structure will function during works and be restored to pre-bridge construction conditions or better. If deficiencies are identified, alternative work should describe how deficiencies in flood protection/structure will be overcome.
- c) It is expected that the SVCA will be provided with elevation surveys of pre-conditions as a marker for post conditions.

Temporary Crossing over Main Saugeen:

Limited information has been provided to the SVCA on the proposed temporary crossing and how this crossing will impact the flood control dyke. Based on the Letter of November 25, 2019, the temporary modular bridge will be needed for at least a year and will need to penetrate the top portion of the dyke. Part 1 above of this letter speaks to the project addressing any short- or long-term impacts on the control of flooding.

Given the height of the dyke at this location relative to the surrounding parking lot (behind arena), it is hard to envision how high the temporary bridge will be and possibly how deep the cut or lowering of the top of the dyke at this location will be in order to get traffic safely back to road grade.

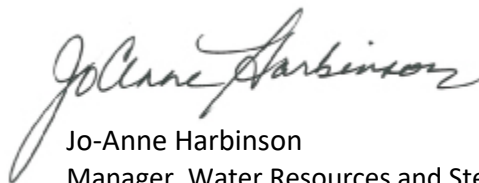
The effectiveness and integrity of the dyke is of utmost concern to the SVCA at this location as the two rivers come together and take a hard turn to the east. Any lowering of the dyke surface will increase the risk of flooding in Paisley and render the active dyke system unreliable/compromised. Repair of the dyke to original state may require additional work in order to provide the same level of flood protection. The SVCA will have to be satisfied as to how that will be achieved.

Please keep SVCA staff informed as the project progresses. If you have any questions, please do not hesitate to contact the undersigned.

Sincerely,



Elise MacLeod
Regulations Officer
Saugeen Conservation



Jo-Anne Harbinson
Manager, Water Resources and Stewardship Services
Saugeen Conservation

B. M. Ross and Associates Limited

RE: Class Environmental Assessment, Replacement of the Teeswater River Bridge

January 16, 2020

Page 5 of 5

EM & JH/

cc: Jim Donohoe, County of Bruce (via email)
Kerri Meier, County of Bruce (via email)
Mark Davis, Authority Member, SVCA (via email)

June 17, 2020

B.M. Ross and Associates Limited
62 North Street
Goderich, Ontario
N7A 2T4

Cc: County of Bruce

Re: Class EA for the Replacement of the Teeswater River Bridge

Dear Ms. Vader,

The mission of the Grey Bruce Health Unit is to work with the Grey Bruce community to protect and promote health while valuing equitable opportunities that support well-being for all. A strategic priority and primary public health concern for the Health Unit is injury prevention. Interactions between motor vehicles and pedestrians/cyclists can cause serious injuries and remains a local concern. Using evidence-informed practice, the Grey Bruce Health Unit is responsive to the needs of the health unit's population and uses the best available evidence to address any emerging issues.

Based on the project options, the Grey Bruce Health Unit has concerns with both Proposed Detour Option 1 and 2B. These two options will direct large amounts of traffic on roads inhabited by a number of Mennonite families. Both options will also direct travel past two Mennonite schools (Cedar Echo and Wildwood). A large number of children walk and cycle to these locations daily and high volumes of traffic may impose a safety risk for these families and their students. Limited availability of accessible shoulders on these portions of road may further compound safety concerns. We believe it would be very difficult for both motor vehicles and pedestrians to share this road space safely.

Due to the perceived safety risk, we are requesting additional consultation occur directly with the populations who may be affected. Please note, some members of these communities may not readily respond to traditional means of outreach.

There are also concerns that Proposed Detour Options 1 and 2 would eliminate any opportunity for local residents to travel north or south within the Village of Paisley. This would include students or residents who typically travel actively to school, work or for leisure. Students who reside north of the bridge, and previously may have walked to school, would now incur a long bus ride as part of the detour.

Transportation planning decisions have a major effect on the health and safety of a community. As such, the Grey Bruce Health Unit is recommending the County of Bruce consider Proposed Detour Option 3 to ensure the health and safety of our communities.
A healthier future for all.

Sincerely,

A handwritten signature in black ink, consisting of a series of loops and a long horizontal stroke extending to the right.

Ian Reich
Program Manager
Grey Bruce Health Unit



MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT FOR REPLACEMENT OF THE TEESWATER RIVER BRIDGE IN PAISLEY - COUNTY OF BRUCE

NOTICE OF VIRTUAL PUBLIC MEETING

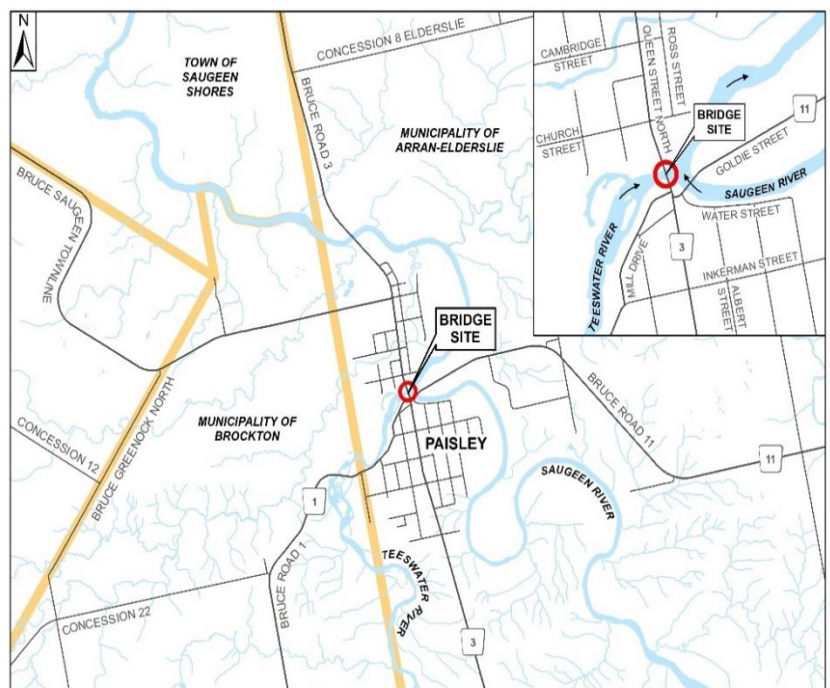
THE PROJECT

The County of Bruce is planning to replace the bridge spanning the Teeswater River in central Paisley on Bruce Road 3 (see attached key plan). The bridge was constructed in 1935 and has reached the end of its service life. The new crossing will be constructed in the same location as the existing and be designed to accommodate flooding events in the Saugeen and Teeswater Rivers. As part of the Class EA process, residents will have an opportunity to provide input on design features associated with the bridge including, sidewalk width, railing style, etc.

Detour options during construction are also being considered as part of study investigations. Due to the size of the crossing, construction could take up to one year. Four detour options have been posted on a website dedicated to the project to allow residents to provide feedback on the various detour alternatives. The Class EA process is expected to be completed by the fall of 2021, with construction of the new crossing tentatively scheduled to begin in 2022.

THE ENVIRONMENTAL ASSESSMENT PROCESS:

The planning for this project is following the environmental planning and design process set out for Schedule 'C' activities under the Municipal Class Environmental Assessment (Class EA) process, which is an approved process under the Environmental Assessment Act. The purpose of the Class EA process is to identify any potential environmental impacts associated with the proposed works and to plan for appropriate mitigation of any identified impacts. This process includes consultation with the general public, government review agencies, indigenous communities and affected property owners.



PUBLIC INVOLVEMENT:

Public consultation is a key component of this study. A **Virtual Public Meeting** has been scheduled to advise residents of the current status of the project and to receive additional input from interested parties. Details of the meeting are as follows:

Date: Tuesday September 22, 2020
Time: 6:00 to 8:00 p.m.
Format: Virtual Meeting

Due to Covid-19 concerns, the meeting will be held virtually using the zoom platform. Pre-registration is required to participate during the meeting. Presentation material will be posted on the project website at www.paisleybridgestudy.ca as of September 14, 2020 to allow residents an opportunity to review the material in advance. Representatives from the County of Bruce and the project engineers, will be present at the meeting to answer questions. Please contact Lisa Courtney at lcourtney@bmross.net or (888) 524-2641 x-238 to register for participation in the meeting.

Please submit your comments on the presentation material to the project engineers: B.M. Ross and Associates: 62 North Street, Goderich, Ontario, N7A 2T4. Telephone (Toll Free): (888) 524-2641. Kelly Vader, Environmental Planner (e-mail: kvader@bmross.net). If you are unable to access the presentation material on-line, please contact BMROSS and alternative arrangements will be made.

Comments collected in conjunction with this project will be maintained on file for use during the project and may be included in project documentation. With the exception of personal information, all comments will become part of the public record.

This Notice Issued September 9, 2020

September 15, 2020

Review Agency
(see attached list)

**RE: Class EA to Replace the Teeswater River Bridge
County of Bruce (Paisley)**

The County of Bruce is planning to replace the bridge spanning the Teeswater River in central Paisley on Bruce Road 3 (see attached key plan). The bridge was constructed in 1935 and has reached the end of its service life. The new crossing will be constructed in the same location as the existing and be designed to accommodate flooding events in the Saugeen and Teeswater Rivers. As part of the Class EA process residents will have an opportunity to provide input on design features associated with the bridge, including sidewalk width, railing style, etc.

Detour options during construction are also being considered as part of study investigations. Due to the size of the crossing, construction could take up to one year. Four detour options have been posted on a website dedicated to the project to allow residents to provide feedback on the various detour alternatives. The Class EA process is expected to be completed by the fall of 2021, with construction of the new crossing tentatively scheduled to begin in 2022.

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Your organization has been identified as possibly having an interest in this project and we are soliciting your input. A **Virtual Public Meeting** has been scheduled to advise residents of the current status of the project and to receive additional input from interested parties. Details of the meeting are as follows:

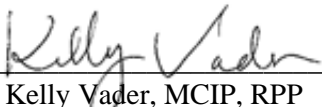
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Alternatively, we can forward you hard copies of the presentation material to review. Please forward any comments on the information by October 30, 2020. If you have any questions or require further information, please contact the undersigned at kvader@bmross.net or by phone at 1-888-524-2641.

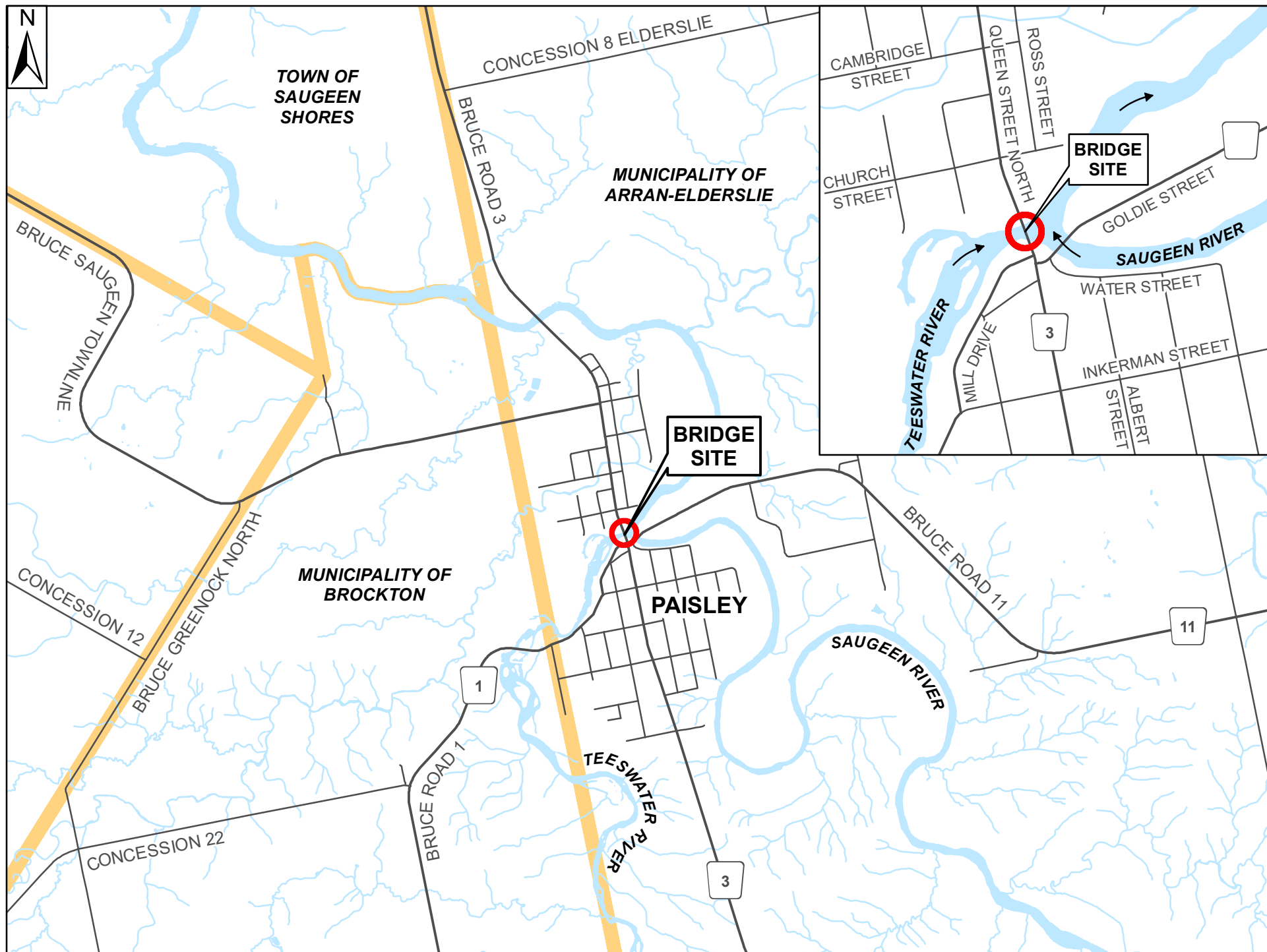
Yours very truly

B. M. ROSS AND ASSOCIATES LIMITED

Per 
Kelly Vader, MCIP, RPP
Environmental Planner

KV:hv
Encl.

cc. Jim Donohoe, County of Bruce



**COUNTY OF BRUCE
CLASS EA TO REPLACE PAISLEY BRIDGE
PROJECT: 13127**

REVIEW AGENCY CIRCULATION LIST

REVIEW AGENCY	
Ministry of the Environment, Conservation & Parks (London) - EA Coordinator	Mandatory Contact
Ministry of Natural Resources and Forestry - Guelph	Potential Impact on Natural Features
Ministry of Tourism, Culture and Sport – Toronto neil.mackay@ontario.ca	Potential Impact to Cultural Heritage Features
Ministry of Transportation (Owen Sound)	Potential Impact on Transportation Network
Municipality of Brockton John Strader CRS-I (jstrader@brockton.ca)	Adjacent Municipality
Municipality of Kincardine Don Huston (dhuston@kincardine.ca)	Adjacent Municipality
Town of Saugeen Shores Amanda Froese (amanda.froese@saugeenshores.ca)	Adjacent Municipality
Bruce County - Planning & Development Department JVanDorp@brucecounty.on.ca	- Proponent - General Information
Grey Bruce Health Unit Jason Weppler (J.Weppler@publichealthgreybruce.on.ca)	Impact on Health
Saugeen Valley Conservation Authority	Potential Impact on Natural Features
Municipality of Arran-Elderslie Scott McLeod (works@arran-elderslie.ca)	Adjacent Municipality
Department of Fisheries and Oceans Canada	Burlington
Bruce Grey Catholic District School Board Transportation Services	Impact on Transportation
Bluewater District School Board	Impact on Transportation

September 15, 2020

Aboriginal Community
(see attached list)

**RE: Class EA to Replace the Teeswater River Bridge
County of Bruce (Paisley)**

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Detour options during construction are also being considered as part of study investigations. Due to the size of the crossing, construction could take up to one year. Four detour options have been posted on a website dedicated to the project to allow residents to provide feedback on the various detour alternatives. The Class EA process is expected to be completed by the fall of 2021, with construction of the new crossing tentatively scheduled to begin in 2022.

The planning for this project is following the environmental planning and design process set out for Schedule 'C' activities under the Municipal Class Environmental Assessment (Class EA) process, which is an approved process under the Environmental Assessment Act. The purpose of the Class EA process is to identify any potential environmental impacts associated with the proposed works and to plan for appropriate mitigation of any identified impacts. This process includes consultation with the general public, government review agencies, indigenous communities and affected property owners.

Your community has been identified as possibly having an interest in this project. A **Virtual Public Meeting** has been scheduled to advise project stakeholders of the current status of the project and to receive additional input from interested parties. Details of the meeting are as follows:

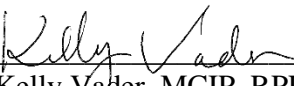
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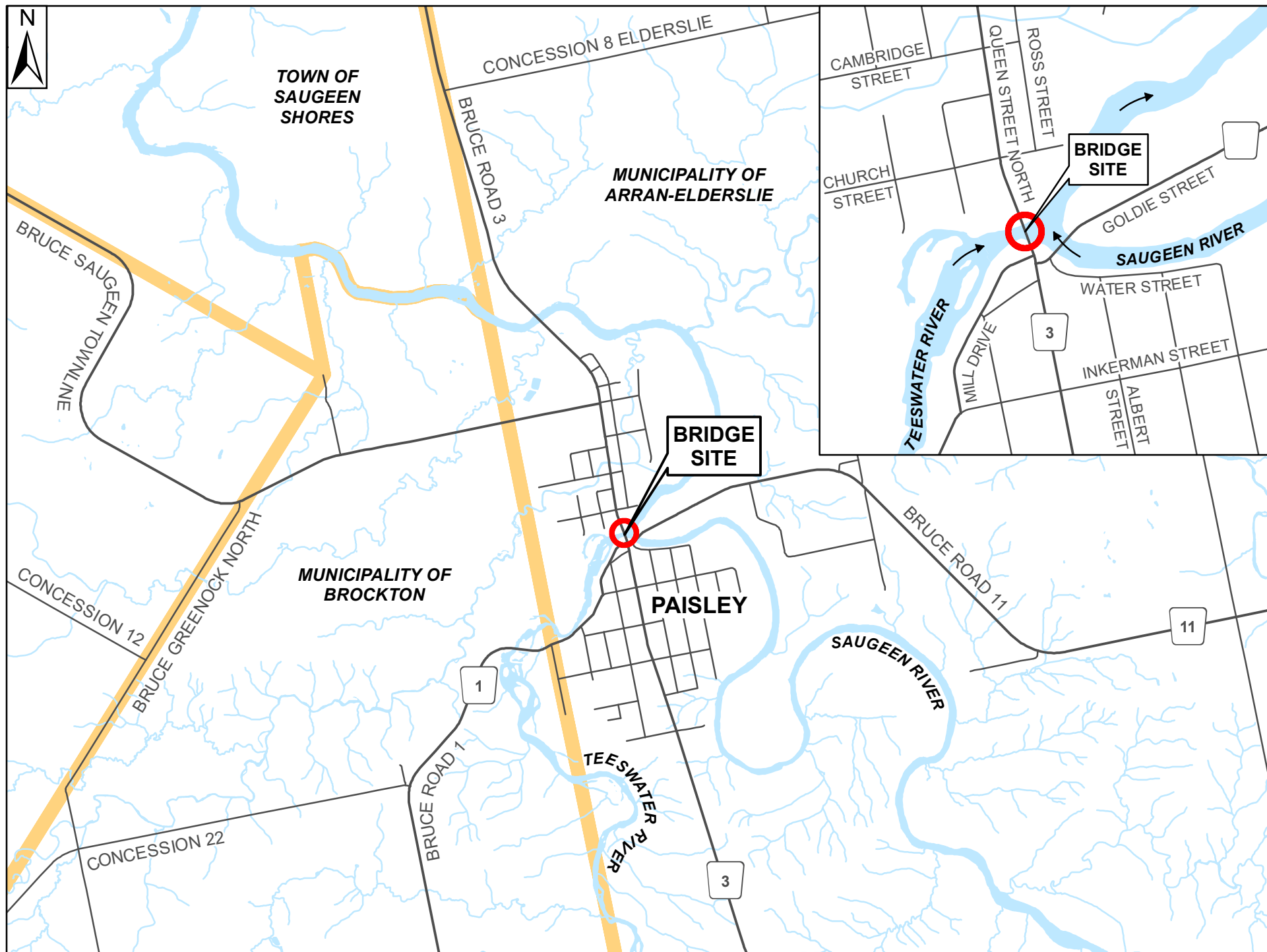
Yours very truly

B. M. ROSS AND ASSOCIATES LIMITED

Per 
Kelly Vader, MCIP, RPP
Environmental Planner

KV:hv
Encl.

cc. Jim Donohoe, County of Bruce



**COUNTY OF BRUCE
CLASS EA TO REPLACE PAISLEY BRIDGE
PROJECT: 13127**

ABORIGINAL CIRCULATION LIST

Chippewas of Nawash Unceded First Nation
Chief Gregory Nadjiwon
R.R. #5
Wiarton, ON N0H 2T0
executiveassistant@nawash.ca

Chippewas of Saugeen First Nation
Chief Lester Anoquot
Hwy. 21, R.R. # 1
Southampton, ON N0H 2L0
sfn@saugeen.org

Saugeen Ojibway Nation (SON)
Chippewas of Saugeen & Chippewas of Nawash
Land Use Planning: Juanita Meekins
25 Maadookii Subdivision
Neyaashiinigmiing, ON N0H 2T0
juanita.meekins@saugeenojibwaynation.ca

Historic Saugeen Métis
George Govier, Consultation Coordinator
204 High Street, Box 1492
Southampton, ON N0H 2L0
saugeenmetisadmin@bmts.com

Métis Nation of Ontario
Suite 1100 – 66 Slater Street
Ottawa, ON K1P 5H1
consultations@metisnation.org

Great Lakes Métis Council
380 9th Street East
Owen Sound, ON N4K 1P3
greatlakesmetis@gmail.com

COUNTY OF BRUCE

CLASS ENVIRONMENTAL ASSESSMENT

TEESWATER RIVER BRIDGE REPLACEMENT



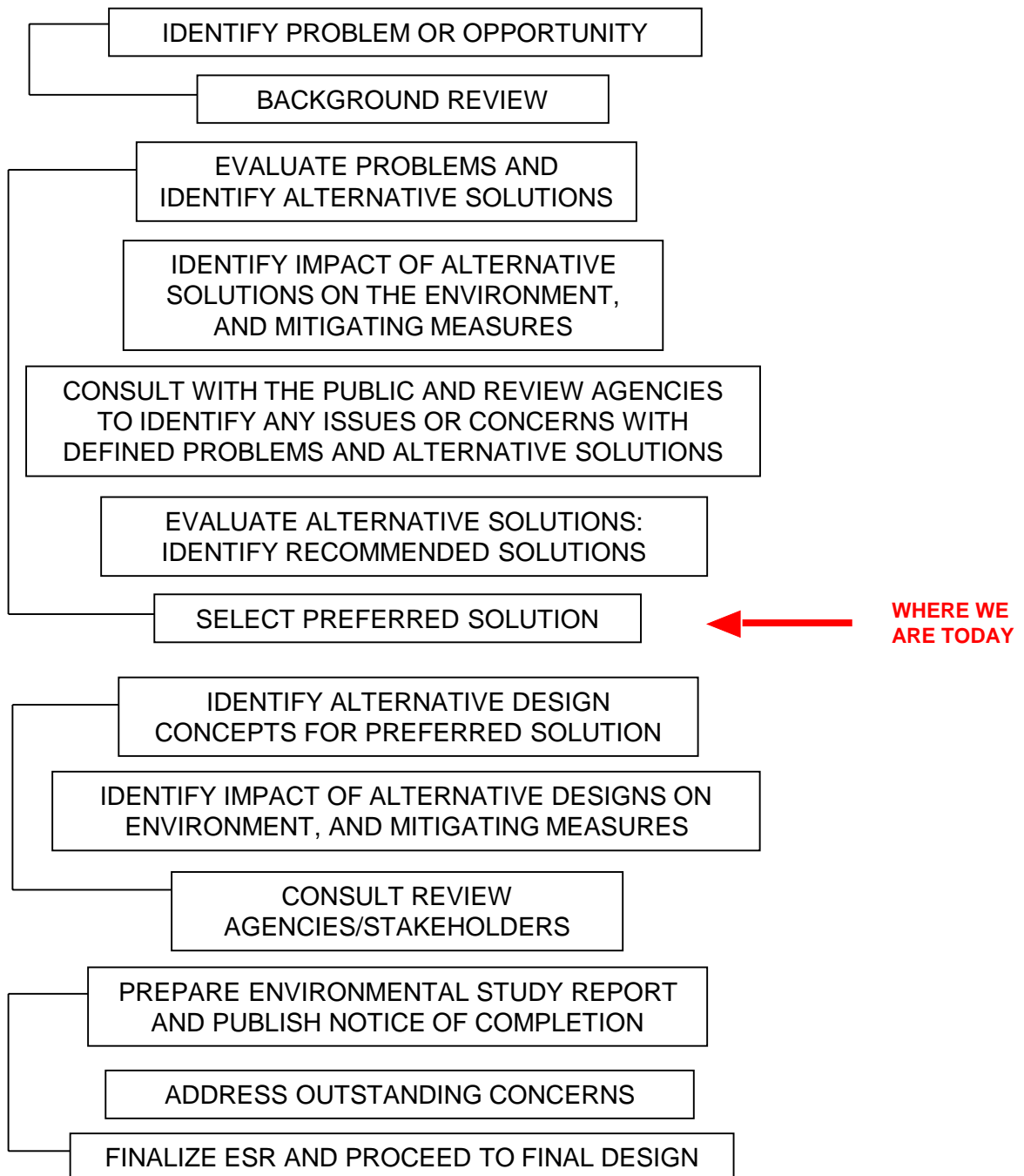
WELCOME

VIRTUAL PUBLIC MEETING

SEPTEMBER 22, 2020



CLASS EA STUDY PROCESS (PHASES 1 -5)



MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT

SUMMARY OF CLASS EA PROCESS:

- PLANNING AND DESIGN PROCESS FOR MUNICIPAL WATER, ROAD AND WASTEWATER PROJECTS
- CONDUCTED TO EVALUATE THE POTENTIAL IMPACTS OF THE PROJECT ON THE NATURAL, CULTURAL, SOCIAL, ECONOMIC, AND BUILT ENVIRONMENTS

STUDY PHASES:



SCOPE OF THIS STUDY:

- RECONSTRUCTION OR ALTERATION OF A STRUCTURE WHEN THE STRUCTURE IS OVER 40 YEARS OLD, WHICH AFTER APPROPRIATE EVALUATION IS FOUND TO HAVE CULTURAL HERITAGE VALUE (< 2.4 M)
 - SCHEDULE C PROJECTS APPROVED SUBJECT TO COMPLETION OF FULL CLASS EA PROCESS (PHASES 1 THRU 5)
- GENERAL STUDY COMPONENTS:
 - DEFINE PROBLEM / OPPORTUNITY;
 - IDENTIFICATION OF ALTERNATIVE SOLUTIONS;
 - CONSULTATION WITH THE PUBLIC / REVIEW AGENCIES;
 - SELECTION OF A PREFERRED ALTERNATIVE;
 - EVALUATION OF ALTERNATIVES / IMPACT MITIGATION;
 - PREPARATION OF ENVIRONMENTAL STUDY REPORT (ESR); AND
 - FINAL PUBLIC NOTIFICATION.



PROJECT TIMELINES

October 2019 – Initial Public/Agency Notifications

Winter 2020 – Cultural Heritage Report Completed

May 2020 – Signs erected at bridge advertising web site

June 2020 – Aquatic Habitat Assessment Completed

September 2020 – Public Information Meeting

Fall 2020 – Hydrological Assessment to be Completed

Winter 2021– Preliminary Bridge Design to be Completed

Spring 2021 – Second Public Meeting

Summer 2021 – Finalize Class EA/Publish Notice of Study
Completion and Environmental Study Report

Spring 2022 – Start of Construction

CULTURAL HERITAGE EVALUATION

CHARACTER-DEFINING HERITAGE ATTRIBUTES:

➤ CURVED CONCRETE T-BEAM DESIGN

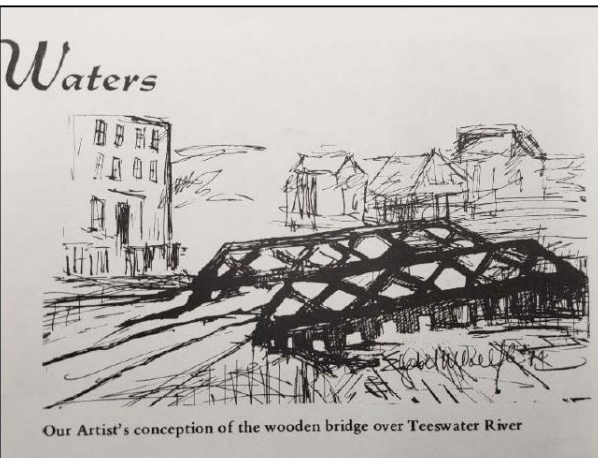
“The bridge is a rare and early example of a curved concrete T-beam bridge in Ontario, which retains its original design features and is notable for its three continuous spans”



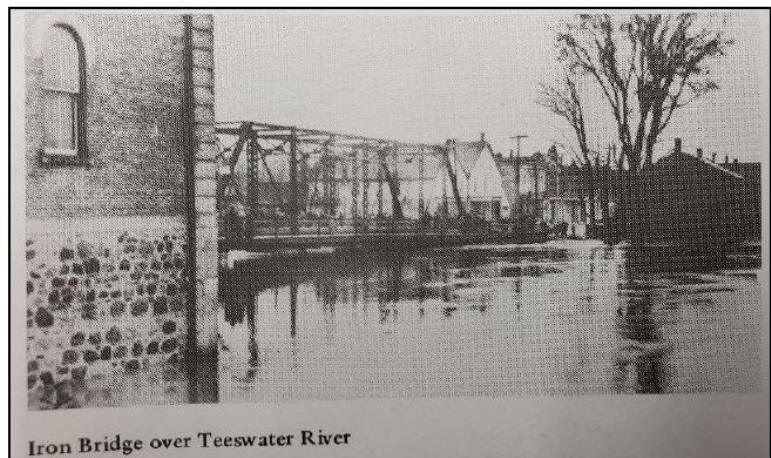
View looking north at bridge piers



View of southeast abutment at connection to Town Hall



Wooden Bridge Image



Iron Bridge that preceded current structure

TEESWATER RIVER BRIDGE

DEFICIENCIES:

- AGE; CONSTRUCTED IN 1935 – 85 YEARS
- DECK DETERIORATION; SEVERE SCALING, CONCRETE SPALLING AND DELAMINATION, EXPANSION JOINTS
- SIDEWALK SOFFITS BADLY DETERIORATED

SIDEWALK SOFFITS



BEAM DETERIORATION

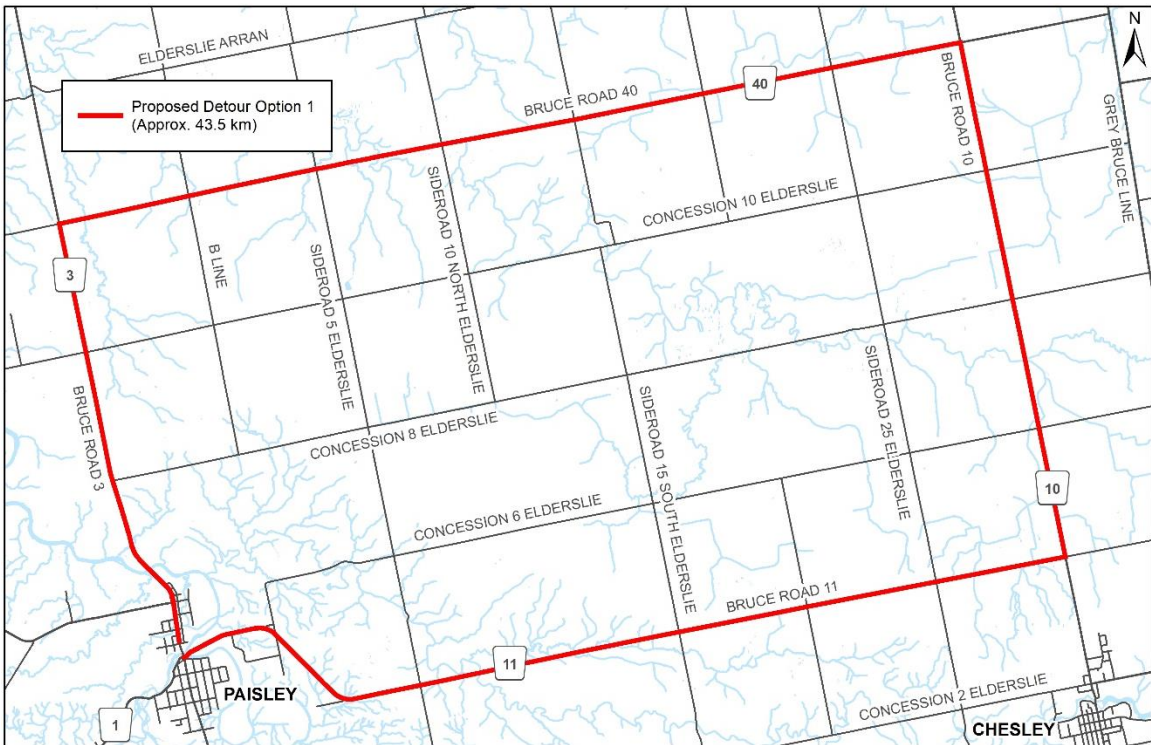


EAST GIRDER

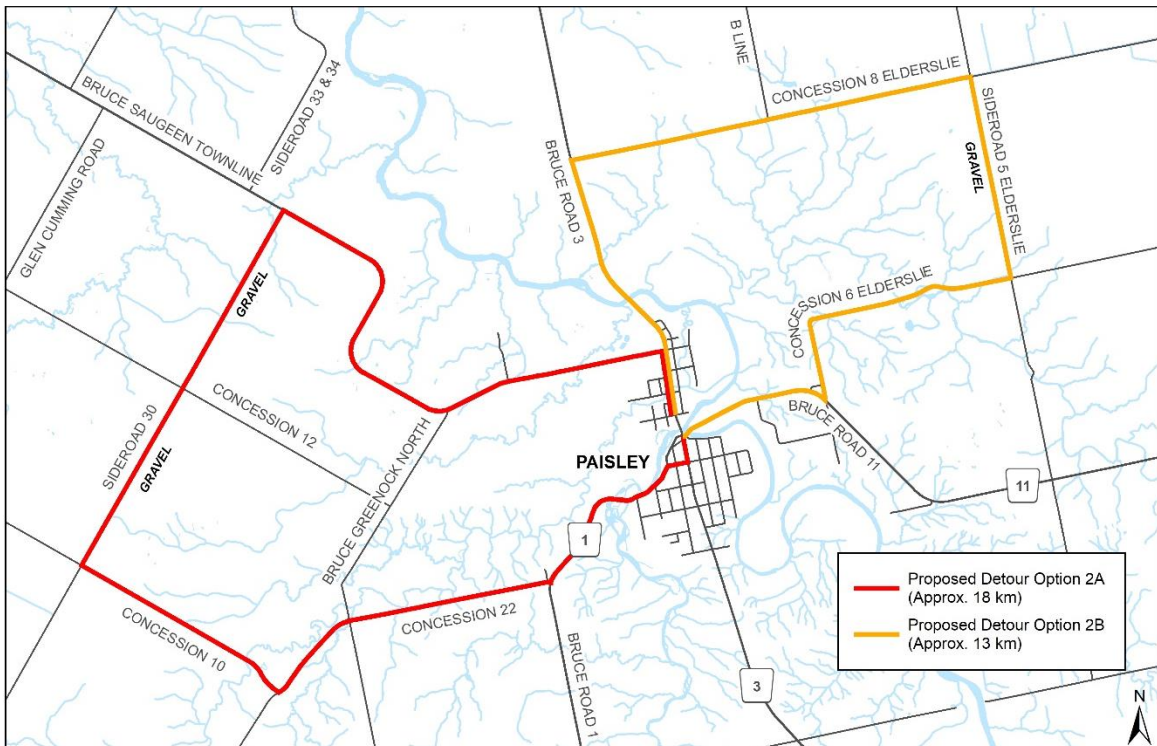


EXPANSION JOINT

PROPOSED DETOUR OPTIONS



COUNTY ROAD DETOUR – 43.5 KM

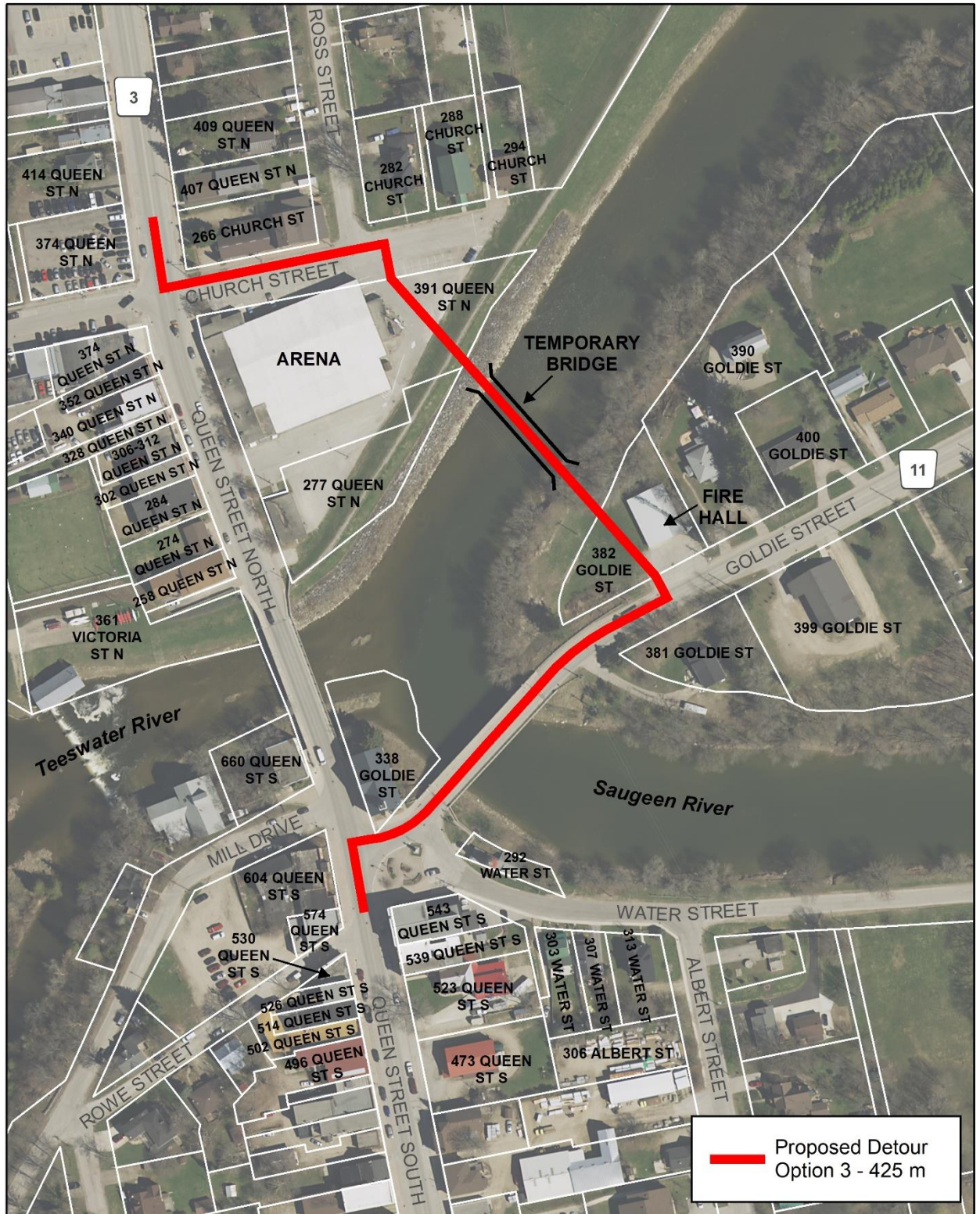


WEST – 18 KM

LOCAL ROAD DETOURS

EAST – 13 KM

PROPOSED DETOUR OPTIONS



IN-TOWN DETOUR

Potential Impacts

- Social Environment
 - Access During Construction
 - Noise/Vibrations
 - Impacts to Businesses
- Economic Environment
 - Capital Construction Costs
- Cultural Environment
 - Cultural Heritage
- Natural Environment
 - Terrestrial Habitat
 - Species at Risk/Fish Habitat
 - Flooding



ACCESS OVER RIVER DURING CONSTRUCTION



MUSSEL HABITAT - RAINBOW MUSSEL

FISH HABITAT



FLOODING IMPACTS





Arran-Elderslie Municipal Newsletter

Fall 2020

Mayors Message

It is hard to believe how fast fall is upon us and with the change in seasons, we also have kids starting back in school. This is a great time to remind everyone to be extra careful and make sure we slow down on our roads. Entering and exiting towns and villages is especially important and can be forgotten with everything else on our minds right now. Several of our residents have invested in lawn signs to help with awareness and should be thanked for their efforts. Arran Elderslie's council is encouraged by Bruce County's traffic calming measures in Invermay and will implement similar measures in our community, this fall.

Please stay safe and vigilant as we get through this pandemic together

Steve Hammell
Mayor of Arran-Elderslie

Leaf Collection Days

Two pickup days for leaves and compost are scheduled for Chesley, Paisley and Tara. Please place your leaves and compost out on October 27th & November 10th for pickup.

Chesley Landfill

The Chesley Landfill is open every Saturday from 8 a.m. to 12 noon in the months of October and November. No residential garbage is permitted. Only unpainted wood, tires, brush and metal will be accepted at this location. There is no charge for wood and brush at the Chesley Landfill.

Taxes

Final Tax bills were mailed out in July.

Due dates: August 26th & October 26th

We offer a number of methods for you to pay your property taxes.

You can pay your taxes by mail, dropping off your payment at the Municipal Office, setting up internet or telephone banking with the Municipality of Arran-Elderslie as a payee, or by preauthorized monthly or installment payments for property taxes or Paisley water and sewer payments. Please note a receipt will not be sent in the mail unless you include a self-addressed stamped envelope. Payment options are also included on the back of your tax bill.

did you know?

Municipal Office

Please note that the Municipal Office in Chesley is currently closed due to COVID-19 restrictions. There are drop-off boxes for you to drop off documents or payments inside the front door. You will not be able to enter the office. Staff can still be reached by telephone or email and are still committed to offering great service.

Road Construction

Chesley

The County of Bruce in partnership with the Municipality of Arran-Elderslie Public Works Department is beginning construction on 1st Ave N in Chesley starting August 31st with a completion date of October 30, 2020. The project will take place between 2nd St. NE. and 4th Street N and will consist of new water and sanitary mains, water and sewer services, storm sewer, sidewalk, and asphalt replacement. The road profile and parking in the area will also change slightly. 2nd St NE and 2nd Ave NE will become a detour route for automobile traffic. Please note that the Chesley downtown business are still open and you can follow this detour to the downtown to support our local businesses.

Paisley

The Municipality of Arran-Elderslie Public Works Department is beginning construction on Nelson Street in Paisley in August and September 2020. The project will take place between Inkerman and Alma Street and will consist of a new water main installation on the West side of the road, as well as removal of storm sewer to install ditching along both sides of the road. The road profile will also change significantly, as the road will be straightened and the width increased as well.

Burn Permits

You must apply for an open-air burn permit for agricultural and recreational fires. The permit is valid for one calendar year. You can fill out the form online on the Municipal website or obtain a paper copy at the Municipal office.

If you have submitted a permit online and included an email address, you will receive your permit number by email which approves your permit request. If you submit a permit request using a paper form or do not include your email address, you must call the Municipal office to obtain your permit number.

Dog Tag Application Info

All dogs must be registered in the Municipality of Arran-Elderslie. A dog tag must be attached to the dog's collar at all times. If you get a new dog please call the Municipal Office to register your dog and to purchase a tag. It is your responsibility to notify the office if there is any change to your pet's information, you are moving out of the Municipality, or you no longer have a dog. You can pay for your dog tag fee by cash, cheque or through online banking. For information on how to pay using online banking, visit our website under 'Animal Services'

Arena Board Advertising

Anyone wishing to advertise or cancel their current advertising on the Arena Boards is asked to contact Katrina at 519-363-3039 ex 117, prior to the start of the ice season.

Compost Bins in Paisley

There are compost bins in Paisley located at the water tower. Please do not place garbage in these compost bins. These bins will be taken away if abused.

Recreation Master Plan

The Municipality is undertaking a Recreation Master Plan process to create a multi-year strategy for recreation, facilities and leisure services. This process will involve community involvement in many ways including a community forum and public survey to ensure that residents have an opportunity to voice their thoughts on recreation services within the Municipality. Visit our website or Facebook page for updates. For more information, contact Carly at recreation@arran-elderslie.ca

Online Building Permits

You can now submit and manage your building permits online. Find the link on the Arran-Elderslie website under the 'Building and Renovating' page. You can also download a step by step guide for applying to permits online.

Our Building department works with builders and owners to ensure building codes are met to make sure that every structure built in Arran-Elderslie is built safely. A building permit must be obtained prior to any new structure, addition or renovation. [You can apply for a permit online.](#)

Need Help? [Download a Step-by-Step illustrated guide](#) to apply for a building permit.

Frequently Asked Questions

+

Where do I start?

-

How do I obtain a permit?

1. Complete an Application for a Permit to Construct or Demolish or pick up a paper copy at the Municipal Office

2. Include the following documents:

- A site plan
- A lot grading and drainage plan (where applicable)
- Plans and specifications (digital or paper)

3. Submit your completed paper application to the Municipal Office

OR

Submit your completed permit through [Arran-Elderslie Online E-Permitting](#) at the online kiosk at the Municipal Office

OR

Submit your completed permit through [Arran-Elderslie Online E-Permitting](#) on your home computer. Need Help? [Download a Step-by-Step illustrated guide](#) to apply for a building permit.

Blood Donor Clinic

Blood Donor Clinic at the Chesley Community Centre on September 4th 2020. **Give the gift of life.**

Ice Time Available

Contact us early if you are looking for ice times in Paisley, Tara or Chesley for the upcoming season. Limited times are available. Book early!

Drop-In Programs

Unfortunately, due to Covid 19 restrictions, there will be no drop in programming available at this time. All programs must be registered for in advance. Attendance will be taken each week.

Cost for programs will remain as they were at \$2 per visit, and you can show up as often or as little as you like.

Program start dates will be determined, and all registrants will be contacted with the information. To preregister for any of the following programs, please email programs@arran-elderslie.ca or call 226-974-0156 before September 18th.

- Tai chi (Chesley)
- Morning fitness (Chesley)
- Walking group, (Chesley, Paisley, Tara)
- Carpet Bowling (Chesley, Paisley)

Paisley Bridge Public Meeting

The County of Bruce is planning to replace the bridge spanning the Teeswater River in central Paisley on Bruce Road 3. The bridge was constructed in 1935 and has reached the end of its service life. The new crossing will be constructed in the same location as the existing and be designed to accommodate flooding events in the Saugeen and Teeswater Rivers.

As part of the Class EA process, residents will have an opportunity to provide input on design features associated with the bridge including, sidewalk width, railing style, etc. **A Public Meeting is being organized for September 22, 2020 from 6:00 to 8:00 p.m., to allow residents an opportunity to review current study progress and to provide additional input on the project.**

Due to Covid19 concerns, the meeting is being held virtually. Presentation material will be posted on the project website at www.paisleybridgestudy.ca as of September 14, 2020 to allow residents an opportunity to review the material in advance. Representatives from the County of Bruce and the project engineers, will be present at the meeting to answer questions. Please contact Lisa Courtney at lcourtney@bmross.net or (888)-524-2641 x-238 to register for participation in the meeting.

Virtual Community Information Session

Chesley Emergency Department – Update on Reduced Hours of Operation

Wednesday, September 9 @ 6:30 PM

If you have a computer and internet access, you can participate by Zoom video conference, or watch a live stream on SBGHC's Facebook page. You can also join by telephone.

To register and for instructions on how to connect, please contact:
patientrelations@sbghc.on.ca or

(519) 370-2400 ext. 2214




**SOUTH BRUCE GREY
HEALTH CENTRE**
CHESLEY | DURHAM | KINCARDINE | WALKERTON





County of Bruce

Class EA for Replacement of the Teeswater River Bridge





Virtual Public Meeting
September 22, 2020

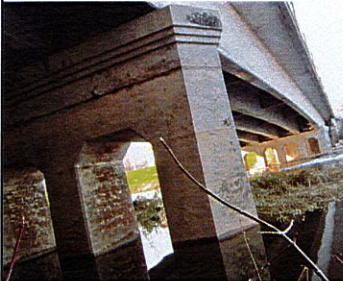

Agenda

- Project Background
- Schedule 'C' Class EA Process
- Specialized Studies
- Detour Alternatives
- Bridge Alternatives
- Next Steps





Teeswater River Bridge


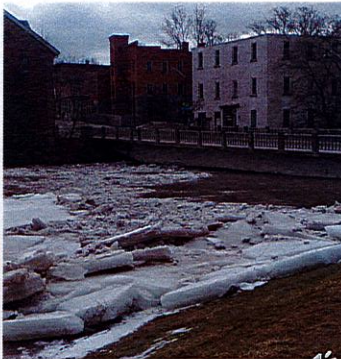
- Three span T-Beam Girder Bridge
- Constructed Circa 1935

- Deficiencies
 - Concrete Deterioration
 - Flood Capacity
 - Deck Deterioration




Teeswater River Bridge - Deficiencies

Concrete deterioration

Flood Capacity

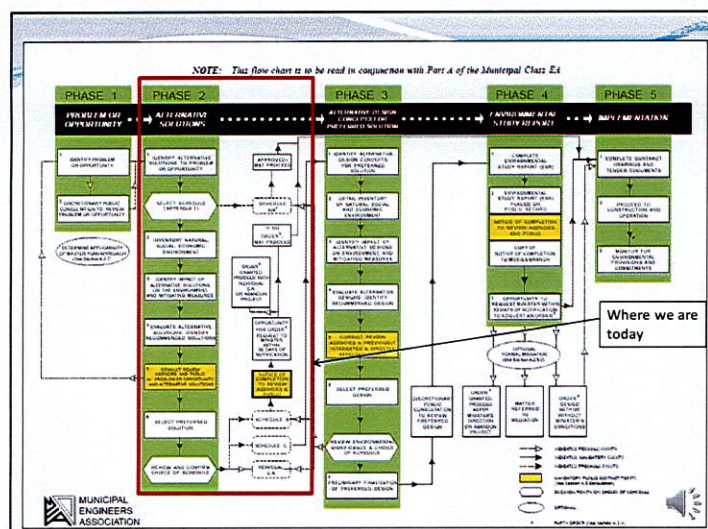
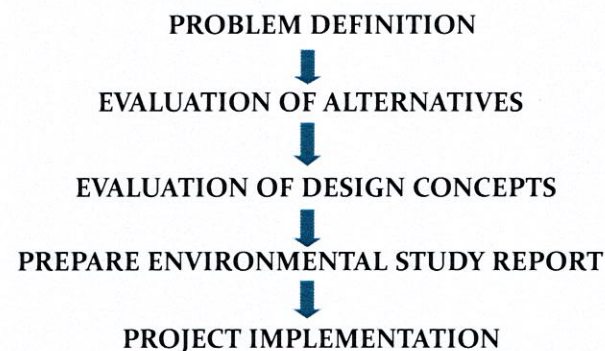


Municipal Class Environmental Assessment (Class EA)

- Planning and Design Process for Municipal Water, Wastewater and Road Projects
- Conducted to Evaluate the Potential Impacts of Municipal Projects and Impact Mitigation
- Involves Consultation with the Public, Regulatory Agencies, Adjacent Property Owners
- Requires Consideration of Natural, Social, Cultural, Economic and Built Environments



CLASS EA STUDY PHASES



Class EA Consultation Program

- October 2019 – Project Initiation
 - Notice Published in Local Papers (Sun Times, Paisley Advocate)
 - Letter sent to Review Agencies and Stakeholders
 - Notice sent to Adjacent Property Owners
 - Letter sent to Aboriginal Communities
- May 2020 – Website launched with Signs at Bridge
 - Dedicated website created for the project
 - Opportunity for residents to provide additional input
 - Opportunity for travelers to provide input
 - More than 60 submission through the website



Input from Residents

- Comments Related to the New Bridge Design
 - Wider sidewalk would be preferred
 - Possible viewing platform to view river and dam
 - Appearance of bridge should reflect Paisley, not the standard
- Comments Related to Longer Detour during Construction
 - Concerned with impacts to downtown businesses – loss of tourist traffic, already impacted by Covid19
 - Concerned with emergency response time
 - General social impacts to residents who work and live in town or have children in school
 - Local detour poses potential risk to Mennonite community
 - Temporary bridge would be preferred



Input from Agencies

- **Ministry of Environment, Conservation and Parks**
 - Adequate consultation must occur with residents, stakeholders, First Nation and Metis Communities
 - Impacts associated with Climate Change and Source Water Protection need to be considered
- **Saugeen Valley Conservation Authority (SVCA)**
 - Ontario Regulation 169/06, Development, Interference with Wetlands, and Alterations to Shorelines and Watercourses.
 - Concerned with potential impacts to flow regime in the river that could aggravate flooding
 - SVCA owns and maintains flood control dyke which is incorporated into current bridge design



Input from Agencies

- **Bruce County Planning Dept.**
 - Encourage bridge design that supports the Paisley Community identity as the “Artistic River Village”, and provides for a high-quality pedestrian experience
 - Suggest coordination with redevelopment of the Paisley Inn property, located at 604 Queen Street
- **Ministry of Heritage, Sport, Tourism, and Culture Industries**
 - Concerns related to archaeological resources, built heritage resources, and cultural heritage landscapes
 - Potential impacts should be screened as part of EA process and be documented in the final report



Input from Agencies

- **Grey Bruce Health Unit**
 - A strategic priority and primary concern of the health unit is injury prevention.
 - Concerned with interactions between vehicles and cyclists/pedestrians.
 - Potential impact to Mennonite Community from detour routes 1 and 2B due to presence of Mennonite schools along route
 - Concerned with impacts to residents from out-of-town detours and would prefer Detour Option 3 be selected



Class EA Bridge Alternatives

- **Alternative 1** – Replacement in the same location
 - Remove existing bridge and construct a new bridge in the same location
- **Alternative 2** – Replacement in a modified location
 - Evaluate bridge location to see if slight modifications to the location can be incorporated into the design to improve road alignment, flooding impacts, etc.
- **Alternative 3** – Do Nothing



Recommended Bridge Alternative:

- Alternative 1 – Replace Bridge in the Same Location

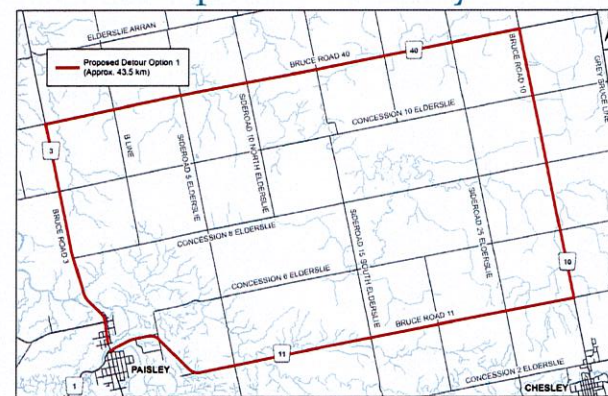


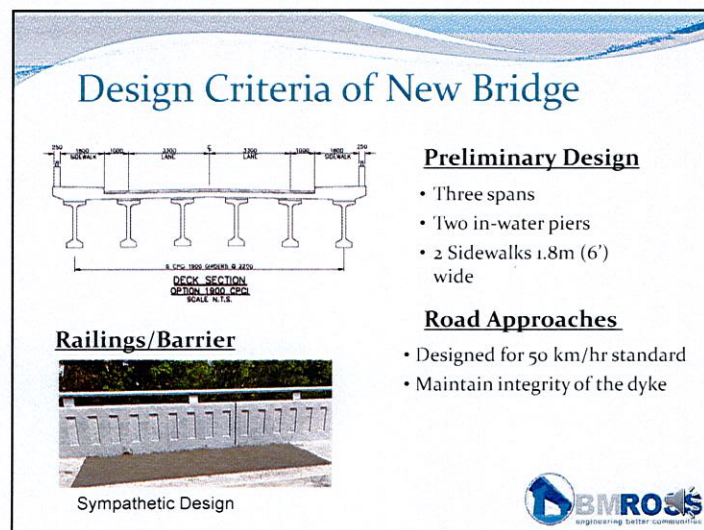
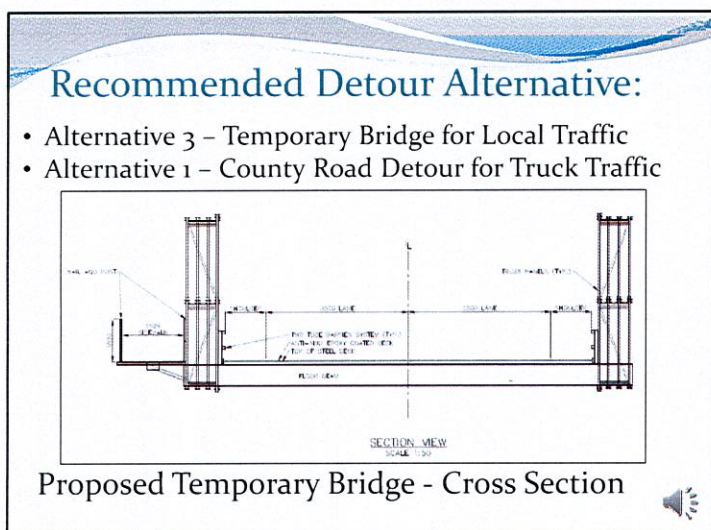
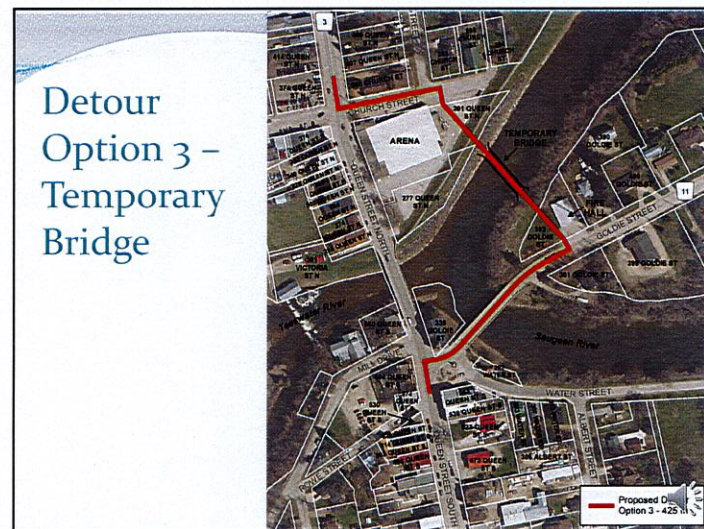
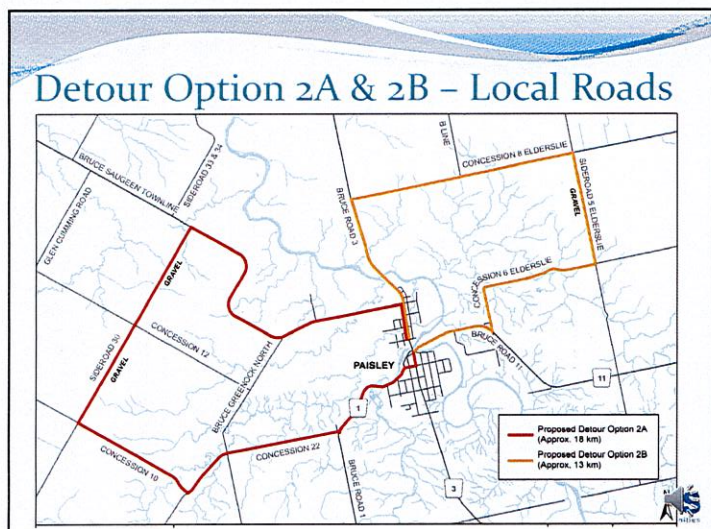
Class EA Detour Alternatives

- **Alternative 1** – Detour using County Roads
 - Formal detour route would follow County Road network
- **Alternative 2A & B** – Detour using local roads
 - There is an east and a west option. East is in Arran-Elderslie and west is in Brockton and Kincardine
- **Alternative 3** – Detour in-town using temporary bridge.
 - New steel panel bridge would be constructed adjacent to the fire hall and exit past the arena with two lanes for traffic and a pedestrian walkway.



Detour Option 1 – County Roads





Specialized Studies

• Natural Environment – Species at Risk

- Characterization Study completed by Natural Resource Solutions Inc. (NRSI) on June 30, 2020
- Assessed river at bridge and possible temporary bridge for sensitive habitat features and mussels
- Examined terrestrial habitat adjacent to bridge and on river banks at temporary bridge location
- Fresh water mussels are present at the site and will need to be moved from impacted areas prior to construction.
- In-water work should be timed to avoid fish spawning period from March 15 to July 15
- Bridge removal should be timed to avoid impacts to nesting birds



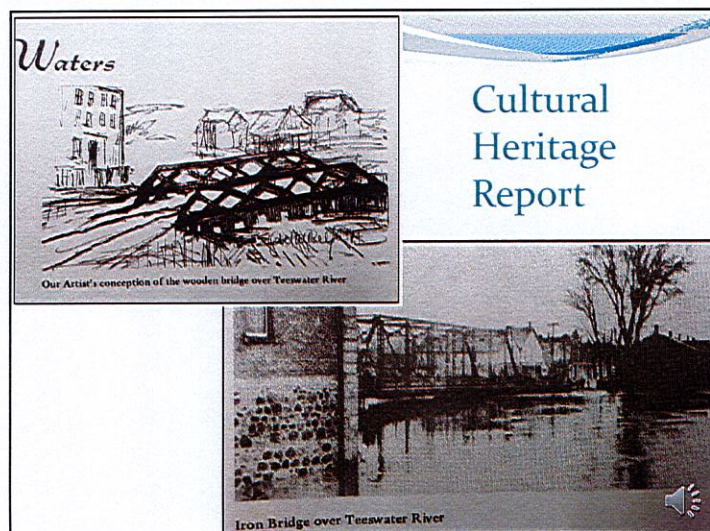
Specialized Studies

• Built Cultural Heritage

- Cultural Heritage Evaluation Report (CHER) completed by Timmins Martelle Heritage Consultants in June 2020
- First bridge built over the Teeswater in 1851, slightly east of current alignment – wooden bridge
- A two span iron bridge replaced this structure in 1895
- Current bridge replaced the iron bridge in 1935

Statement of Cultural Heritage Value

"The bridge is a rare and early example of a curved concrete T-beam bridge in Ontario, which retains its original design features and is notable for its three continuous spans"



Specialized Studies – Cultural Heritage

• Impacts Associated with Construction

- Removal of the bridge will result in a loss of the cultural heritage value represented by the structure
- Options to retain, repair or reuse portions of the structure are not technically feasible, due to the age and condition of bridge

• Potential Mitigation Measures

- Site Documentation with particular attention paid to the curved T-Beam design, through photos or drawings
- Railing of the new bridge could be designed to be sympathetic to the existing railing appearance
- Other design features could be incorporated into new bridge



Additional Investigations

- **Hydrologic Investigation**

- Flood control dyke system installed in cooperation with SVCA, Province and Arran-Elderslie
- Any potential impacts to dyke system would need to be carefully evaluated
- Cut and fill assessment required to assess impacts of temporary detour route on floodplain
- Floodplain mapping is dated and would need to be updated to model the proposed structure
- New bridge design would need to be evaluated to ensure that flooding impacts are not aggravated by the new structure
- Ice jam impacts to be assessed



Proposed Schedule

- **Fall/Winter 2020:**

- Complete Hydrologic Investigation/Consult with SVCA
- Complete Preliminary Bridge Design
- Refine Temporary Bridge Design and Location

- **Spring 2021 – Public Information Meeting #2**

- Present Updated Reports and Investigations
- Present Preliminary Bridge Design and Features

- **Summer 2021 – Finalize EA Process & Publish Report**

- **Fall 2021 – Complete Engineering Design & Apply for Approvals (DFO/SVCA/MECP)**

- **Spring 2022 – Construction**



Next Steps

- Collect and Review Additional Public Input
- Initiate Additional Discussions with SVCA related to Hydrology and Temporary Bridge
- Start Preliminary Design of New Bridge
- Finalize Design of Temporary Bridge
- Finalize Class EA Environmental Study Report (ESR)
- Publish Notice of Study Completion



Questions?

- Comments or questions on the presentation material can be directed to Kelly Vader at kvader@bmross.net or through the project website at www.paisleybridgestudy.ca
- You can also participate in the Virtual Public Meeting scheduled for September 22, 2020 at 6 pm. Please contact Lisa Courtney at lcourtney@bmross.net to register for the meeting.
- Staff from the County of Bruce and BMROSS will be present at the meeting to answer questions.



COUNTY OF BRUCE

CLASS EA FOR REPLACEMENT OF THE TEESWATER RIVER BRIDGE

PUBLIC MEETING NOTES

Details: Tuesday September 22, 2020
Virtual Public Meeting

Opening Remarks: 6:00 pm - 6:10 pm
Presentation: 6:10 pm – 6:35 pm
Questions: 6:35 pm – 7:10 pm

In Attendance: Miguel Pelletier, Director,
Transportation and Environmental Services) County of Bruce
Jim Donohoe, Engineering Manager)

Steve Hammell, Mayor) Municipality of Arran-Elderslie
Scott McLeod, Public Works)

Andrew Ross) B.M. Ross and Associates (BMROSS)
Kelly Vader)
Lisa Courtney)

Members of the public: 30 ±

6:00 p.m. - 6:10 p.m. – Opening Remarks

- Panelists from Bruce County and BMROSS signed in to the meeting
- Previously registered members of the public signed in to the Zoom meeting after logging on
- Lisa Courtney provided brief opening remarks and explained how the meeting would be operated through the Zoom platform. She then introduced representatives of BMROSS and the County of Bruce in attendance at the meeting.

6:10 p.m. – 6:35 p.m. – Presentation

Power Point Presentation with audio (attached)

-
- Miguel Pelletier, began the presentation by thanking everyone for attending the virtual meeting. He then introduced Andrew Ross and Kelly Vader, from B.M. Ross & Associates Ltd., who provided the audio details associated with the presentation material.
 - Kelly Vader reviewed the agenda for the presentation, which included project background, information on the Class EA process, consultation efforts completed to date, and proposed bridge and detour alternatives.
 - Andrew Ross provided information on the current Teeswater River Bridge, including details on the existing deficiencies present in the structure and specialized features associated with the bridge.
 - Kelly Vader reviewed a flow chart outlining the Municipal Class EA process. She explained the various phases that were included in the EA process and that all 5 phases would be completed for a more complex project like the Teeswater River Bridge project and that the next phase would include a review of detailed design components associated with the bridge design.
 - Kelly discussed the consultation that had been completed as part of the EA process completed to date. She noted that a significant number of comments had been received from residents, particularly after signs were erected at the bridge advertising a website dedicated to the project.
 - Kelly then reviewed input received from agencies, such as Ministry of the Environment, Conservation and Parks, Saugeen Valley Conservation Authority, Ministry of Culture, Bruce County Planning Department and the Grey Bruce Health Unit.
 - Kelly then reviewed the three bridge replacement alternatives that were identified for replacement of the Teeswater River Bridge, being 1) replacement in the same location, which would mean removal of the existing structure, 2) replacement in a slightly modified location, and 3) Do Nothing. Kelly then indicated that due to existing site constraints presented by the rivers and road network, replacement of the bridge in the same location, Alternative 1, was the only reasonable alternative to select for the project.
 - Andrew described the 4 detour alternatives that were identified for the project, including a County Road detour, two local road detours (one east and one west), and an in-town detour that would require construction of a temporary bridge over the Saugeen River. He then showed a cross-section of the temporary bridge which provided details on the lane width and pedestrian walkway. Andrew indicated that the temporary bridge option is very expensive and that a decision on a preferred detour route has not yet been finalized.
 - Andrew also reviewed the preliminary design criteria for the new bridge crossing and explained that the new crossing would likely have three spans, similar to the existing bridge. He explained that design details associated with the railings, barriers and sidewalks would be reviewed in more detail at a later meeting. He showed an example of a barrier from the Chesley Bridge which had indentations that were reminiscent of the former railings.
 - Andrew explained that a Hydrology Study is required for the project and will be completed in the next phase of the Class EA investigations. Existing modeling information has been provided to BMROSS by SVCA. Additional survey work may be required to update the model. BMROSS staff will work closely with SVCA during work on the model to ensure that the new bridge and possible temporary bridge, will not negatively impact flood levels in the community.
 - A proposed schedule to finalize the Class EA was then reviewed along with next steps in the Class EA process. Questions were then invited from members of the public in attendance.

6:35 p.m. – 7:15 p.m. – Questions

After concluding the presentation, questions were invited from those members of the public who had pre-registered to attend the virtual public meeting. Copies of the meeting notes and presentation material will be made available on the Count of Bruce website as well as project website.

Summary of Questions and Answers

- Q. A resident wanted to know the cost of the temporary bridge detour. He was concerned with winter driving conditions for the out of town detours. He realized that the cost question was answered during the presentation.
- A. Andrew Ross added that the cost estimate was prepared based on input from a bridge contractor. The cost will be refined when the design is finalized, so is still an estimate at this time.

- Q. A resident and business owner asked if the temporary bridge could be left as a permanent fixture for use in the Paisley trail system.
- A. Miguel Pelletier indicated that the temporary bridge is intended to be only in place during the project construction and that the components are rented not purchased, so leaving in place would add to the project costs. Andrew Ross added that the bridge looks very industrial and would be excessive if a second pedestrian crossing of the river is all that is wanted by residents.

- Q. Steve Hammell, Mayor of Arran-Elderslie asked if the new bridge could be constructed immediately adjacent to the existing bridge? He also indicated that the in-town detour would be preferred for the community and that Bruce Road 3 is a very busy road serving the entire county not just the community of Paisley.
- A. Andrew Ross indicated that this approach has been used in other locations, but has not been investigated in a lot of detail for the Teeswater River Bridge project due to the presence of the buildings in close proximity to the existing bridge. This approach would also require modifications to the road network which could be very costly.

- Q. A resident asked how long the bridge construction would last.
- A. Andrew Ross indicated that he was anticipating 14 months of construction, including placement of the temporary bridge before the old bridge is removed. He suggested that construction of the new bridge would take approximately 10 to 12 months, when residents would need to use a different route over the river.

- Q. Has there been any consideration given to access to existing buildings during construction of the new bridge.
- A. Andrew indicated that access to existing businesses will be accommodate during construction. There may be brief periods when alternative access arrangements will need to be made.

- Q. A resident expressed concerns over increased traffic east and west of Paisley during construction of the new bridge. The resident noted that many individuals walk and bike to school and expressed a concern about safety on the detour routes.
- A. The concern was acknowledged by the panelists.

- Q. A resident asked if some components of the temporary bridge could be left in place for use by the community to enhance the Artistic Village experience, especially given the costs associated with the temporary bridge.
- A. Andrew Ross indicated that it might be possible to reuse portions of the temporary bridge abutments, but it would require more investigation.

-
- Q. A resident asked if there was municipal infrastructure on the bridge and wondered what the pipes on the side of the bridge are for.
- A. Andrew Ross indicated that he was aware that a watermain was located underneath the river at the bridge. It may not need to be moved as part of the project but we would need to be careful regardless, not to disturb it during construction. There are no sanitary sewers crossing the bridge and the existing pipes are carrying telecommunications. Scott McLeod from Arran-Elderslie was at the meeting and confirmed the information that Andrew provided.
- Q. A resident asked if Arran-Elderslie was planning to complete additional road work during replacement of the bridge and asked if the Fire Hall location was a factor in the location of the temporary bridge.
- A. Scott McLeod indicated that the Municipality was considering the possibility of road work north of the bridge, if it could be completed during construction. He indicated that he was not able to comment on the Fire Hall. Miguel Pelletier added that the County would work with Arran-Elderslie as the project moved forward to accommodate any road reconstruction needs that could be coordinated with the bridge project.
- Q. A resident asked if lighting had been considered for the bridge that would be similar to existing lights in Paisley.
- A. Andrew Ross indicated that lighting would be provided on the new bridge and would be designed to blend with existing lighting in the community. The Municipality would typically identify the lighting to be used on the bridge.
- Q. A resident asked if solid barriers are required on the bridge to minimize salt impacts to the river.
- A. Andrew Ross explained that salt was a concern but not just for the environment. He referred to the photo from the presentation that showed the underside of the sidewalks. The corrosion shown in the photo was due to salt dripping over the edge of the sidewalk and corroding the reinforcing steel. Solid barriers prevent this from occurring and also provide a safety barrier for vehicles. The existing bridge railings do not meet the current bridge code.
- Q. A resident asked if the project was being coordinated with the natural gas installation in Bruce County.
- A. Miguel Pelletier indicated that the County has been in discussions with the natural gas utility and would try to coordinate the projects, but he was unsure of their timeline. If the bridge is constructed before gas is installed, a conduit would be installed on the bridge that would allow the gas lines to be placed after the bridge is constructed.
- Q. A resident asked if there would be impacts to the mill structure located at the southwest end of the bridge.
- A. Andrew Ross indicated that potential impacts to the mill will be considered when designing the new bridge and that he will need to consult with the current owners. Access to the east entrance of the mill building may be limited during some components of the construction, but only for a few months. He indicated that a mill race is located under the building and wondered if it is still required. If so, it would be accommodated as part of the new bridge design.
- Q. A resident commented that the aesthetics of the new bridge are very important to the community and wanted to ensure that the County and other panelists are aware of how valuable this component of the bridge design will be.
- A. Miguel Pelletier indicated that he was aware of this concern and design options for the bridge would be presented at another public meeting later in the Class EA process.

Miguel indicated that we are still early in the Class EA process and there will be more opportunities for comment. Any feedback on the project can be submitted to the project engineers or county representatives at any time in the process. Not just during the formal public meeting.

Miguel added that the cost of the temporary bridge is of concern, however it isn't necessarily a reason to rule out this option.

Miguel questioned whether feedback had been received from emergency services about impacts associated with the project. Kelly indicated that a response had been received from the local Fire Chief via email. Kelly will follow up and ask for additional feedback confirming what the impacts of the out of town detours would have on their response times and how many calls they typically have in a year that would require them to cross the bridge.

Jim Donohoe asked if BMROSS had considered the option of reconstructing the bridge a half at a time. Andy indicated that it had not been evaluated in detail due to the presence of the adjacent buildings and how costly and time consuming that approach would be. He indicated that he could investigate more to confirm that that approach would not be feasible.

7:15 p.m. – Meeting Conclusion

The meeting was concluded at 7:15 p.m. Miguel Pelletier thanked everyone for attending before the panelists left the meeting.

Should there be any errors or omissions to these meeting notes, please notify the undersigned.

Meeting Notes Prepared by
B. M. ROSS AND ASSOCIATES LIMITED

Kelly Vader, Environmental Planner

Kelly Vader

From: Martin, Kevin (OPP) <Kevin.Martin@opp.ca>
Sent: Wednesday, October 7, 2020 11:44 AM
To: Kelly Vader
Cc: Jason Weppler (J.Weppler@publichealthgreybruce.on.ca)
Subject: RE: Class EA for the Replacement of the Teeswater River Bridge

Hi Kelly:

I received a message from Bishop Lloyd Shantz today.

He appreciates the information that you shared (he participated in the teleconference) and advised that he plans to continue following as the construction unfolds.

No concerns were raised at this time from his community.

Kevin

Constable Kevin Martin
South Bruce OPP

From: Kelly Vader [mailto:kvader@bmross.net]
Sent: 10-Sep-20 9:17 AM
To: Martin, Kevin (OPP) <Kevin.Martin@opp.ca>
Cc: Andrew Ross (aross@bmross.net) <aross@bmross.net>
Subject: RE: Class EA for the Replacement of the Teeswater River Bridge

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments in unexpected emails.

Kevin:

I'm sure we could make that work.

Kelly

From: Martin, Kevin (OPP) [mailto:Kevin.Martin@opp.ca]
Sent: September 9, 2020 3:30 PM
To: Kelly Vader <kvader@bmross.net>
Subject: RE: Class EA for the Replacement of the Teeswater River Bridge

Great. Thank you.

They may prefer to meet in a drive shed at an old order property - travelling distances takes some time as an average speed for horse and buggy is 15 km/h.

Will advise as soon as I hear.

From: Kelly Vader [<mailto:kvader@bmross.net>]
Sent: 9-Sep-20 2:08 PM
To: Martin, Kevin (OPP) <Kevin.Martin@opp.ca>
Subject: RE: Class EA for the Replacement of the Teeswater River Bridge

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Hi Kevin:

Yes, we would be fine with that. We could meet in a board room setting with them on one end of the table and staff from the County and BMROSS on the other end.

Kelly

From: Martin, Kevin (OPP) [<mailto:Kevin.Martin@opp.ca>]
Sent: September 9, 2020 2:03 PM
To: Kelly Vader <kvader@bmross.net>
Subject: RE: Class EA for the Replacement of the Teeswater River Bridge

Hi Kelly:

I dropped off the meeting notice to Bishop Shantz, but haven't heard back.

If they take part by telephone, that would be good.

Would you be available for a socially-distanced meeting with the elders – if they ask?

Kevin

From: Kelly Vader [<mailto:kvader@bmross.net>]
Sent: 4-Sep-20 3:29 PM
To: Martin, Kevin (OPP) <Kevin.Martin@opp.ca>
Subject: RE: Class EA for the Replacement of the Teeswater River Bridge

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments in unexpected emails.

Hi Kevin:

Please see the attached Notice which will be published in the local papers next week.

Due to concerns with Covid19 we are holding a virtual meeting and will have presentation material available in advance for review. If this format poses a problem for the community, we can provide hard copies of the presentation material for their review and make other accommodations to obtain their feedback.

Please let me know the best way to proceed and I will work on the details.

Kelly Vader, MCIP, RPP
B. M. Ross and Associates Limited
Engineers and Planners
62 North Street

Goderich, ON N7A 2T4

Ph: (519) 524-2641

C: (519) 525-2170

kvader@bmross.net

<https://link.edgepilot.com/s/ff393a55/PdxwYodasESZX6MldggguQ?u=http://www.bmross.net/>

From: Martin, Kevin (OPP) [<mailto:Kevin.Martin@opp.ca>]

Sent: June 25, 2020 3:00 PM

To: Kelly Vader <kvader@bmross.net>; Jason Weppler <J.Weppler@publichealthgreybruce.on.ca>

Cc: Robin Stuempfle <R.Stuempfle@publichealthgreybruce.on.ca>

Subject: RE: Class EA for the Replacement of the Teeswater River Bridge

Hi Kelly:

I made contact with the community today. The health unit already brought this up (Thank you health unit!).

They would like some time to discuss further and I will get a call back when they're ready to talk.

Thank you for reaching out.

Kevin

Constable Kevin Martin
South Bruce OPP

From: Kelly Vader [<mailto:kvader@bmross.net>]

Sent: 25-Jun-20 9:08 AM

To: Jason Weppler <J.Weppler@publichealthgreybruce.on.ca>

Cc: Martin, Kevin (OPP) <Kevin.Martin@opp.ca>; Robin Stuempfle <R.Stuempfle@publichealthgreybruce.on.ca>

Subject: RE: Class EA for the Replacement of the Teeswater River Bridge

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments in unexpected emails.

Hi Jason:

Your assistance is very much appreciated.

Kelly Vader, MCIP, RPP
B. M. Ross and Associates Limited
Engineers and Planners
62 North Street
Goderich, ON N7A 2T4

Ph: (519) 524-2641

Fax: (519) 524-4403

kvader@bmross.net

<https://link.edgepilot.com/s/f40b441f/7uJgQadBi0iYaU876WeABA?u=http://www.bmross.net/>

From: Jason Weppler [<mailto:J.Weppler@publichealthgreybruce.on.ca>]
Sent: June 24, 2020 12:01 PM
To: Kelly Vader <kvader@bmross.net>
Cc: Martin, Kevin (OPP) <Kevin.Martin@opp.ca>; Robin Stuempfle <R.Stuempfle@publichealthgreybruce.on.ca>
Subject: RE: Class EA for the Replacement of the Teeswater River Bridge

[EXTERNAL]: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Kelly,

Sorry for the delayed response.

Kevin Martin, South Bruce OPP, (cc'd here) has offered to connect with the Mennonite community to gauge their concerns. He may be a good point of contact for feedback or can make the appropriate connections as necessary.

We appreciate you investigating this concern more thoroughly.

Jason

From: Kelly Vader <kvader@bmross.net>
Sent: June 19, 2020 8:51 AM
To: Jason Weppler <J.Weppler@publichealthgreybruce.on.ca>
Subject: RE: Class EA for the Replacement of the Teeswater River Bridge

[EXTERNAL]: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Jason:

Thank you very much for the input. Does the Health Unit have a suitable contact with the Mennonite community that we could reach out to? We have received other comments regarding impacts associated with the detour routes and Mennonite schools.

Kelly Vader, MCIP, RPP
B. M. Ross and Associates Limited
Engineers and Planners
62 North Street
Goderich, ON N7A 2T4

Ph: (519) 524-2641
Fax: (519) 524-4403
kvader@bmross.net
<https://link.edgepilot.com/s/f42a8c15/GbT62st2UkatftXYRS5H4w?u=http://www.bmross.net/>

From: Jason Weppler [<mailto:J.Weppler@publichealthgreybruce.on.ca>]
Sent: June 18, 2020 3:33 PM
To: kvader@bmross.net
Subject: Class EA for the Replacement of the Teeswater River Bridge

Hi Kelly,

Please accept the attached comments from the Grey Bruce Health Unit regarding the Class EA for the Teeswater Bridge Replacement in Paisley.

Should you have any questions please feel free to reach out directly.

Jason Wepler
Health Promoter
Grey Bruce Health Unit
519-376-9420 ext. 1408

Please note that the privacy and security of email communication cannot be guaranteed. Please refrain from using email messages to send personal information.

Vision: A healthier future for all.

Mission: Working with Grey Bruce communities to protect and promote health.

Core Values: Effective communication, Partnership, Respectful Relationships, Quality and Innovation, Integrity, Leadership

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MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT FOR REPLACEMENT OF THE TEESWATER RIVER BRIDGE IN PAISLEY - COUNTY OF BRUCE

NOTICE OF VIRTUAL PUBLIC MEETING

THE PROJECT

The County of Bruce is planning to replace the bridge spanning the Teeswater River in central Paisley on Bruce Road 3 (see attached key plan). The bridge was constructed in 1935 and has reached the end of its service life. The new crossing will be constructed in the same location and be designed to improve flow conditions within the rivers.

PUBLIC INVOLVEMENT:

A public meeting was held virtually on September 22, 2020 and included an overview of the project and the Class EA process. A video recording of the previous meeting and presentation materials can be viewed at <http://www.paisleybridgestudy.ca/blog/>.

Public consultation is a key component of this study and a second **Virtual Public Meeting** has been scheduled to update residents of the current status of the project. The meeting will provide additional details of the project, including the preliminary findings of a hydrology study, bridge design alternatives, and discussion of the preliminary preferred detour route. This meeting will also provide residents with the opportunity to provide additional comments on the project. Details of the meeting are as follows:

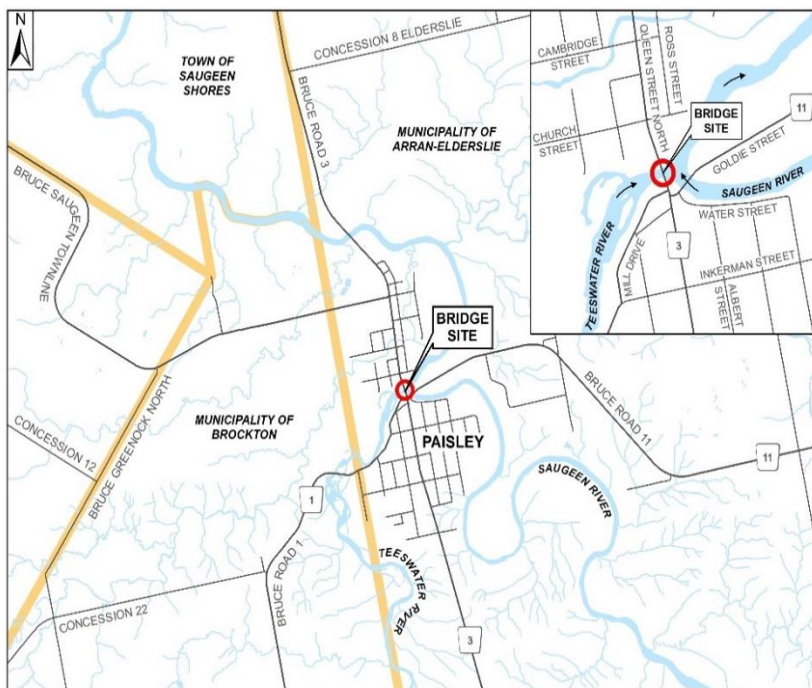
Date: Tuesday May 18, 2021

Time: 6:00 to 8:00 p.m.

Format: Virtual Meeting

THE ENVIRONMENTAL ASSESSMENT PROCESS:

The planning for this project is following the environmental planning and design process set out for Schedule 'C' activities under the Municipal Class Environmental Assessment (Class EA) process, which is an approved process under the Environmental Assessment Act. The purpose of the Class EA process is to identify any potential environmental impacts associated with the proposed works and to plan for appropriate mitigation of any identified impacts. This process includes consultation with the general public, government review agencies, indigenous communities and affected property owners.



Due to Covid-19 concerns, the meeting will be held virtually using the zoom platform. Pre-registration is required to participate during the meeting. Presentation material will be posted on the project website at www.paisleybridgestudy.ca as of May 12, 2021 to allow residents an opportunity to review the material in advance. Representatives from the County of Bruce and the project engineers, will be present at the meeting to answer questions. Please contact Lisa Courtney at lcourtney@bmross.net or (888) 524-2641 x- 238 to register for participation in the meeting.

Please submit your comments on the presentation material to the project engineers: B.M. Ross and Associates: 62 North Street, Goderich, Ontario, N7A 2T4. Telephone (Toll Free): (888) 524-2641. Kelly Vader, Environmental Planner (e-mail: kvader@bmross.net). If you are unable to access the presentation material on-line, please contact BMROSS and alternative arrangements will be made.

Comments collected in conjunction with this project will be maintained on file for use during the project and may be included in project documentation. With the exception of personal information, all comments will become part of the public record.

This Notice Issued April 28, 2021.

Jim Donohoe, P. Eng., Engineering Manager

Second virtual public meeting scheduled for bridge replacement plan on Bruce Road 3



Be an explorer.

By [Fiona Robertson](#)
May 13, 2021 5:48am

Share via:



The County of Bruce is planning to replace the bridge spanning the Teeswater River in central Paisley on Bruce Road 3.

Bruce County says public consultation is a key component of this study and a second Virtual Public Meeting has been scheduled to update residents of the current status of the project.

The meeting will be held virtually over Zoom on Tuesday, May 18th, from 6 p.m. to 8 p.m.


The meeting will provide additional details of the project, including the preliminary findings of a hydrology study, bridge design alternatives, and discussion of the preferred detour route.

This meeting will also provide residents with the opportunity to provide additional comments on the project.



Pre-registration is required to participate during the meeting. Presentation material will be posted on the project website at www.paisleybridgestudy.ca

Their first public meeting was held virtually on September 22, 2020 and included an overview of the project and the Class EA process.

County of Bruce Class EA for Replacement of the Teeswater River Bridge





Virtual Public Meeting
May 18, 2021

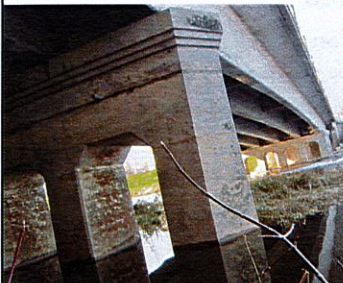

Agenda

- Project Background
- Schedule 'C' Class EA Process
- Hydrological Assessment
- Preliminary Preferred Detour Route
- Bridge Design Alternatives
- Proposed Timelines
- Next Steps





Teeswater River Bridge


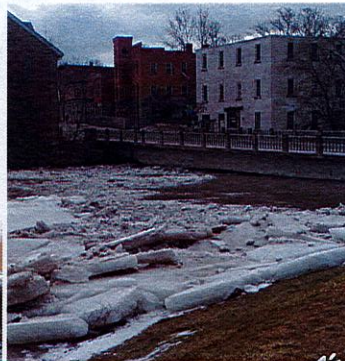
- Three span T-Beam Girder Bridge
- Constructed Circa 1935


- **Deficiencies**
 - Concrete Deterioration
 - Flood Capacity
 - Deck Deterioration



Teeswater River Bridge - Deficiencies

Concrete deterioration Flood Capacity

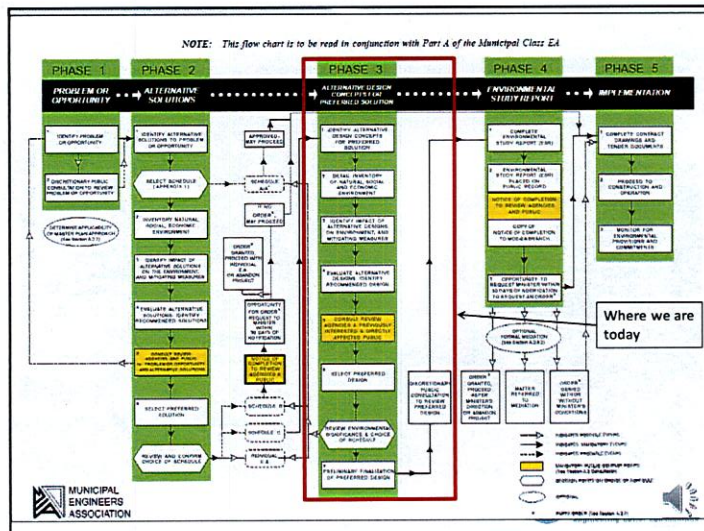
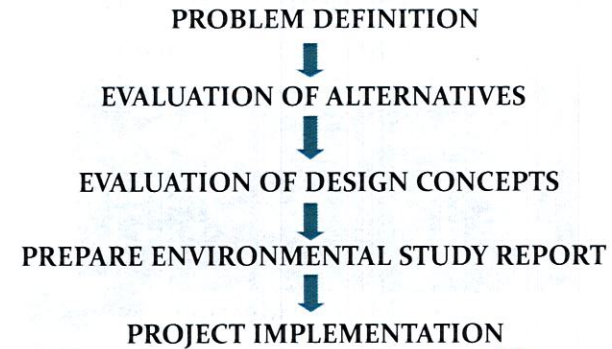


Municipal Class Environmental Assessment (Class EA)

- Planning and Design Process for Municipal Water, Wastewater and Road Projects
- Conducted to Evaluate the Potential Impacts of Municipal Projects and Impact Mitigation
- Involves Consultation with the Public, Regulatory Agencies, Adjacent Property Owners
- Requires Consideration of Natural, Social, Cultural, Economic and Built Environments



CLASS EA STUDY PHASES



Class EA Timelines

- October 2019 – Project Initiation
 - Notice Published in Sun Times, Paisley Advocate
 - Letters sent to Review Agencies, Adjacent Property Owners and Aboriginal Communities
- May 2020 – Dedicated website launched with signs at bridge
- June 2020 – Heritage Evaluation of bridge completed
- June 2020 – Species at Risk Assessment completed
- September 2020 – First Public Information Meeting
- Winter 2021 – Preliminary Bridge Design/Hydrology
- May 2021 – 2nd Public Information Meeting



Input from Residents

- Comments Related to the New Bridge Design
 - Wider sidewalk would be preferred
 - Possible viewing platform to view river and dam
 - Appearance of bridge should reflect Paisley, not the standard
- Comments Related to Proposed Detour Route
 - Concerned with impacts to downtown businesses – loss of tourist traffic, already impacted by Covid19
 - Concerned with emergency response time
 - General social impacts to residents who work and live in town or have children in school
 - Local detour poses potential risk to Mennonite community
 - Temporary bridge would be preferred



Input from Agencies

- **Ministry of Environment, Conservation and Parks**
 - Consultation Program Required
 - Climate Change and Source Water Protection be considered
- **Saugeen Valley Conservation Authority (SVCA)**
 - Concerned with flooding impacts within river
 - SVCA owns and maintains flood control dyke
- **Ministry of Heritage, Sport, Tourism, and Culture Industries**
 - Concerns related to Archaeology, Built & Cultural heritage



Input from Agencies

- **Mennonite Community**
 - Prefer in-town detour route
- **Grey Bruce Health Unit**
 - Concerned with injury prevention, interactions between vehicles & cyclists/pedestrians
 - Potential impact to Mennonite Community from detour
 - Social impacts of longer detour routes
- **Bruce County Planning Dept.**
 - Recommendations on bridge design that reflect community

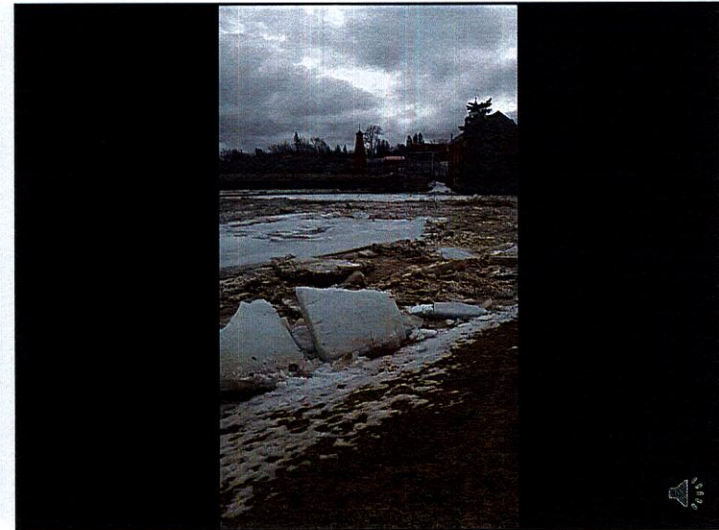
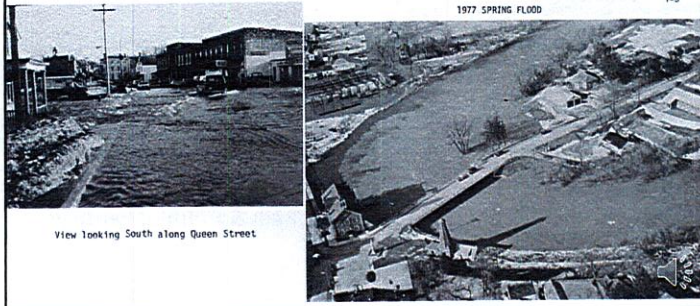


Hydrologic Investigation



Historic Flooding

- Due to Paisley's location at the junction of the Teeswater and Saugeen Rivers, the community is prone to flooding
- There have been a number of historic flooding events in Paisley – 1977 Flood instigated the Flood Control Study (1979)



Hydrologic Investigation

- Previous Flood Control Study recommended that a series of dykes be installed adjacent to the river banks to control flooding within the community
- The existing bridge railing was modified so that the railings would form part of the flood control barrier

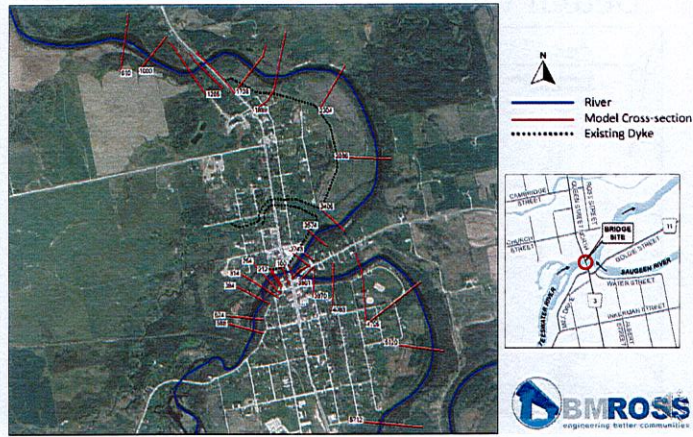


Hydrologic Investigation

- A model of the river was developed during the 1979 study, and last updated in 1990, to simulate conditions in the river during various storm events
- The model has been updated to reflect existing site conditions, with additional topographic information and updated stream gauge records.
- Software used is HEC-RAS
- Information related to the proposed bridge designs, and the temporary detour bridge, has been modeled using the updated HEC-RAS model to ensure that the new bridge will meet floodplain criteria set by the SVCA

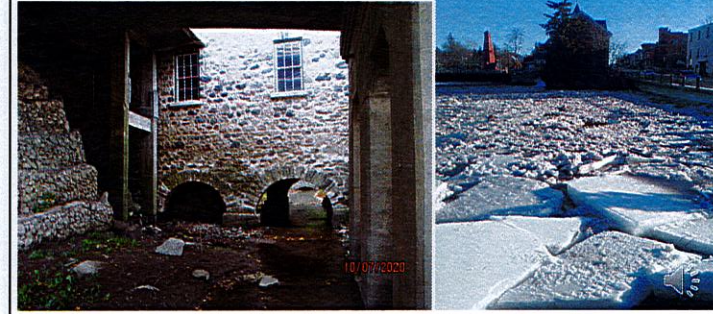


Model of the Saugeen & Teeswater River



Hydrologic Investigation

- **Additional Considerations**
 - Mill Race under Mill Structure
 - Ice Jamming Potential



Hydrologic Findings – Proposed Bridge

- High flood levels are driven by backwater conditions from the larger Saugeen River Flows.
- Proposed structure will improve flood flow and reduce the potential for ice jams. No increase in flooding with proposed bridge structure.
- Historical mill race to be maintained with a culvert within the bridge abutment.
- Rock protection is recommended to eliminate scour at piers.
- Bridge railings are recommended to include heightened barriers for flood protection, to be tied into existing and future dyke upgrades.



Hydrologic Findings – Detour Bridge

- Temporary bridge has been designed for 1:50 year flow, for projected 1 year construction period.
- Low steel elevation has been set to reduce flood impacts. No significant increase in flood levels up to 1:50 year event
- No reduction on existing dyke elevation. Bridge approaches are proposed above existing dyke level.
- Proposed fill within floodplain for approaches is considered insignificant for the river flood storage.
- Rock protection is recommended to eliminate scour at piers

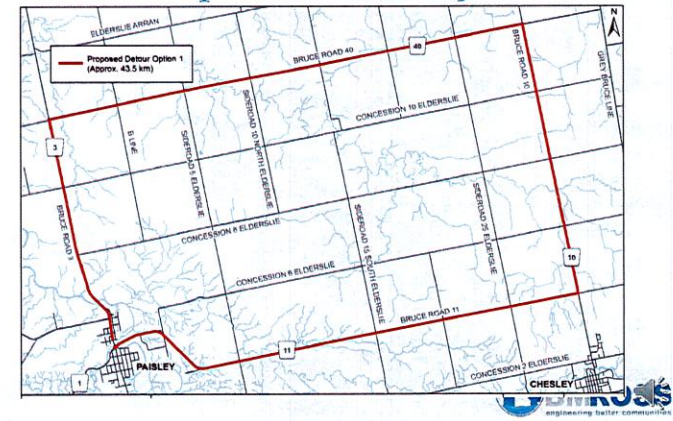


Class EA Detour Alternatives

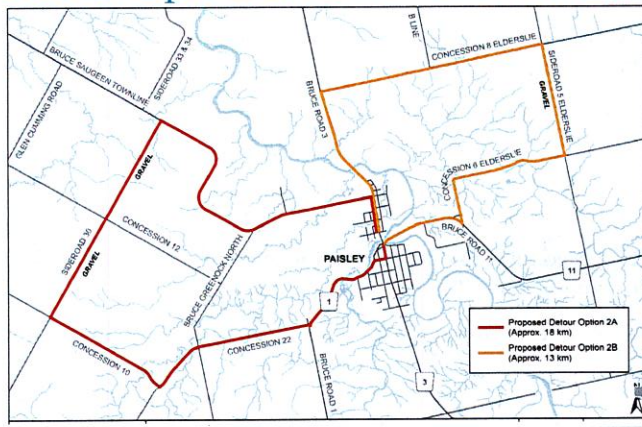
- **Alternative 1** – Detour using County Roads
 - Formal detour route would follow County Road network
- **Alternative 2A & B** – Detour using local roads
 - There is an east and a west option. East is in Arran-Elderslie and west is in Brockton and Kincardine
- **Alternative 3** – Detour in-town using temporary bridge.
 - New steel panel bridge would be constructed adjacent to the fire hall and exit past the arena with two lanes for traffic and a pedestrian walkway.



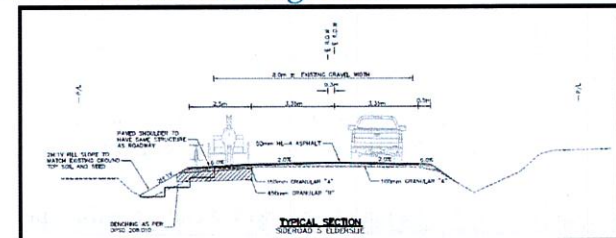
Detour Option 1 – County Roads

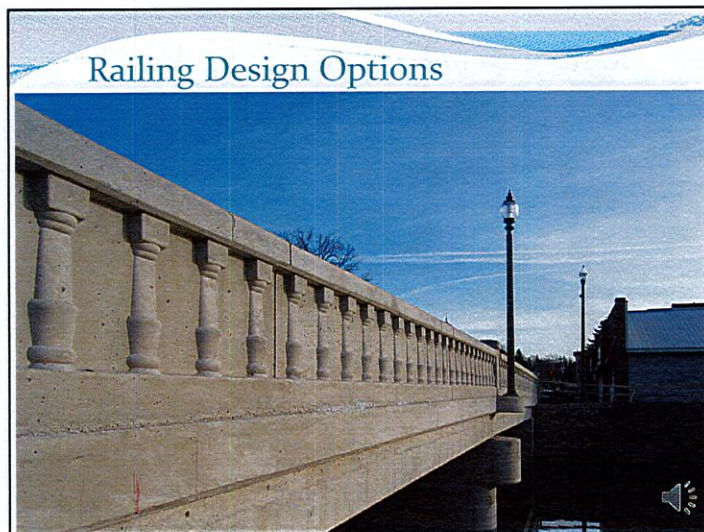
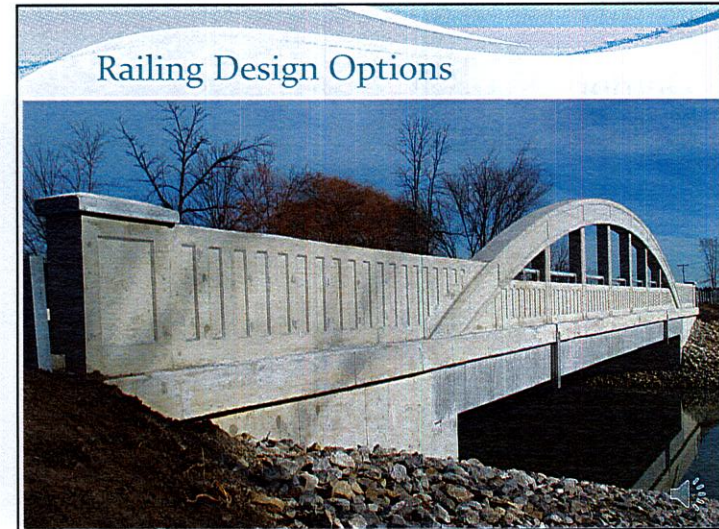
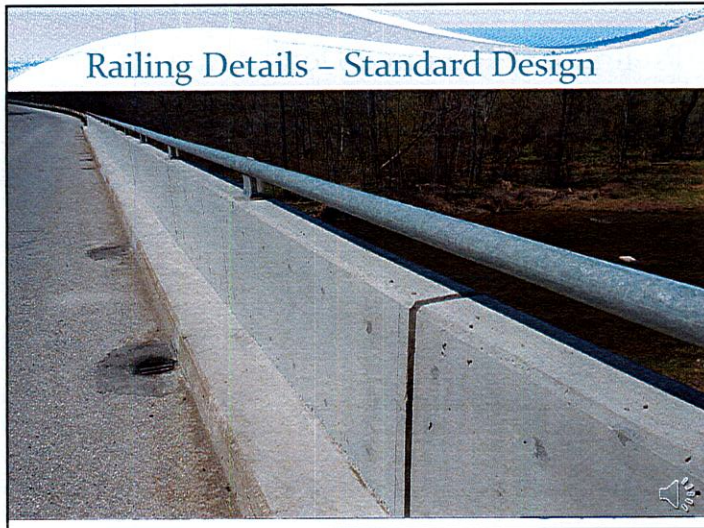


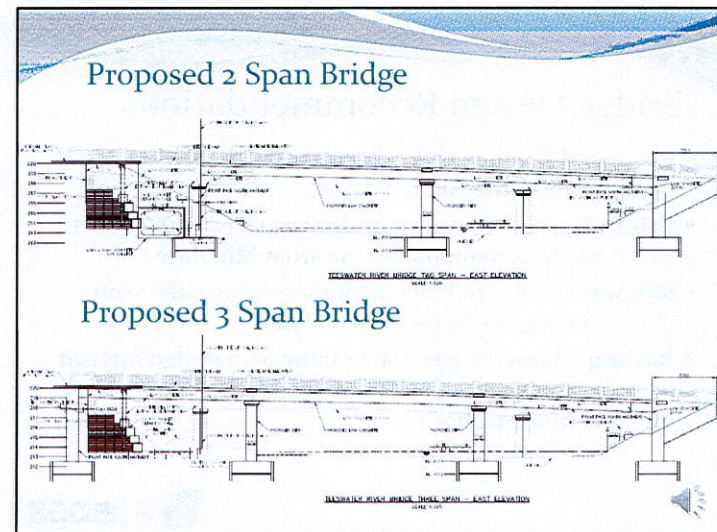
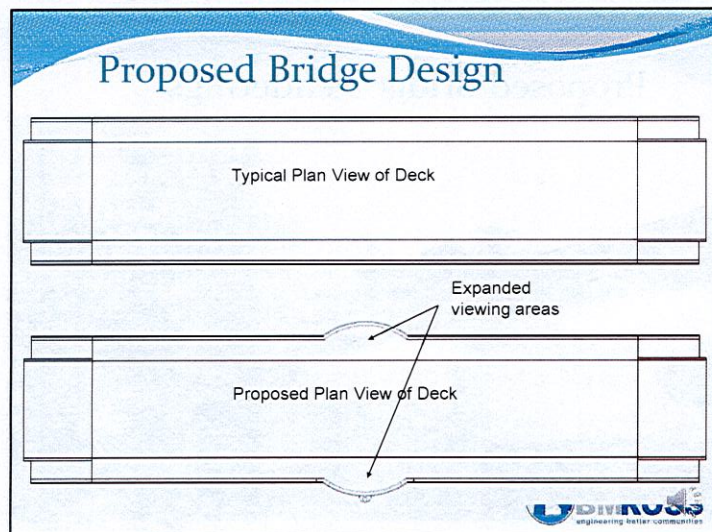
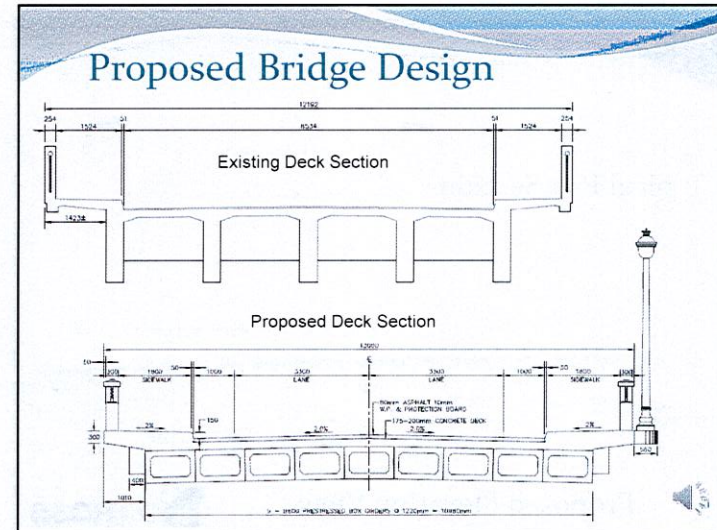
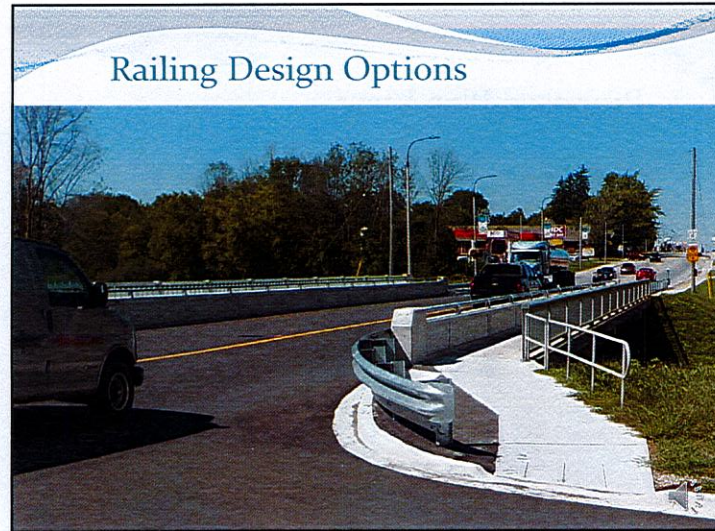
Detour Option 2A & 2B – Local Roads

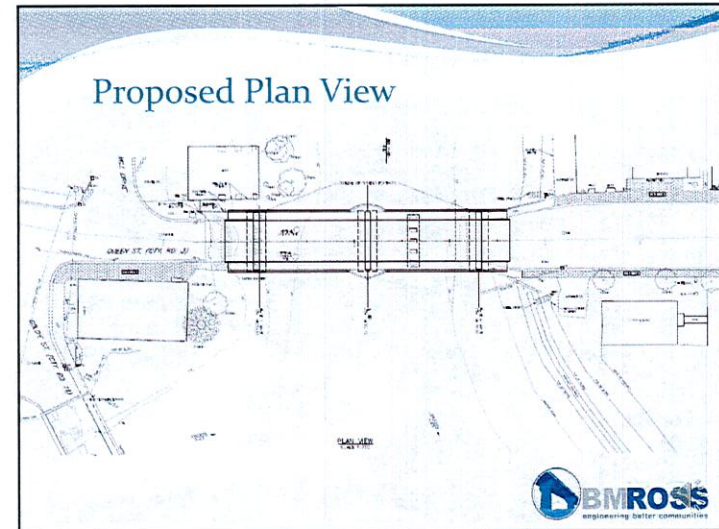
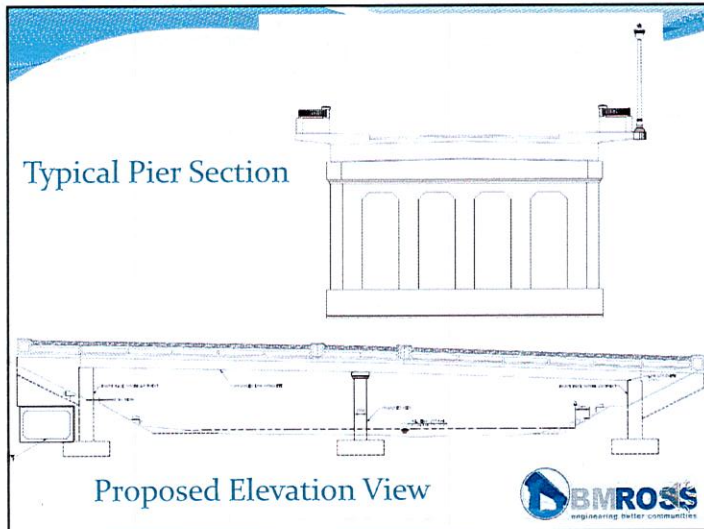


Detour Investigation





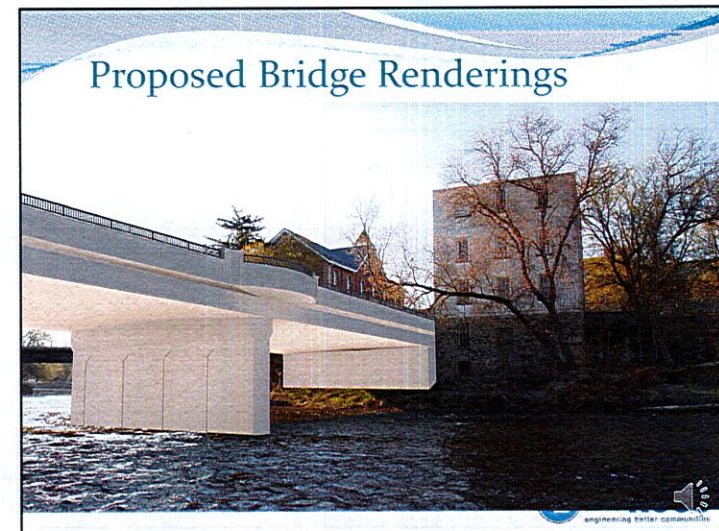




Bridge Design Recommendations

- **Proposed Bridge Deck** – Two lanes with viewing platforms on either side.
- **Bridge Spans** – Two span bridge proposed with culvert at south end to accommodate flow from Mill Race
- **Sidewalk** – 1.8m (6 foot) sidewalk on both sides with wider viewing platforms in the middle
- **Railing** – Lower height solid railing with design imprint to replicate existing + metal railing above, similar to example from Stratford

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engineering better communities



Proposed Schedule

- **Summer/Fall 2021:**
 - Finalize Hydrologic Investigation/Consultations with SVCA
 - Complete Bridge Design
 - Prepare Environmental Study Report (ESR)
- **Fall 2021** – Finalize EA Process & Publish Report
- **Fall 2021** – Complete Engineering Design & Apply for Approvals (DFO/SVCA/MECP)
- **Spring 2022** - Construction



Next Steps

- Collect and Review Additional Public Input
- Confirm Project Details at County Council
- Finalize Discussions with SVCA related to Hydrology and Temporary Bridge
- Finalize Design of New Bridge
- Finalize Design of Temporary Bridge
- Finalize Class EA Environmental Study Report (ESR)
- Publish Notice of Study Completion



Questions?

- Comments or questions on the presentation material can be directed to Kelly Vader at kvader@bmross.net or through the project website at www.paisleybridgestudy.ca
- You can also participate in the Virtual Public Meeting scheduled for May 18, 2021 at 6 pm. Please contact Lisa Courtney at lcourtney@bmross.net to register for the meeting.
- Staff from the County of Bruce and BMROSS will be present at the meeting to answer questions.



COUNTY OF BRUCE
CLASS EA FOR REPLACEMENT OF THE TEESWATER RIVER BRIDGE
PUBLIC MEETING NOTES

Details:

Tuesday May 18, 2021
Virtual Public Meeting

Opening Remarks: 6:00 pm - 6:10 pm
Presentation: 6:10 pm – 6:45 pm
Questions: 6:45 pm – 7:15 pm

In Attendance:

Miguel Pelletier, Director,
Transportation and Environmental Services) County of Bruce
Jim Donohoe, Engineering Manager)

Steve Hammell, Mayor) Municipality of Arran-Elderslie
Mark Davis, Deputy Mayor)
Scott McLeod, Public Works)

Andrew Ross) B.M. Ross and Associates (BMROSS)
Kelly Vader)
Lisa Courtney)

Members of the public: 25 ±

6:00 p.m. - 6:10 p.m. – Opening Remarks

- Panelists from Bruce County and BMROSS signed in to the meeting
- Previously registered members of the public signed in to the Zoom meeting after logging on
- Lisa Courtney provided brief opening remarks and explained how the meeting would be operated through the Zoom platform. She then introduced representatives of BMROSS and the County of Bruce in attendance at the meeting.

6:10 p.m. – 6:45 p.m. – Presentation

Power Point Presentation with audio (attached)

-
- Miguel Pelletier, began the presentation by thanking everyone for attending the second virtual public meeting for the Paisley Bridge project. He then introduced Andrew Ross and Kelly Vader, from B.M. Ross & Associates Ltd., who provided the audio details associated with the presentation material.
 - Kelly Vader reviewed the agenda for the presentation, which included project background, information on the Class EA process, a review of consultation received to date, discussion of detour alternatives, bridge hydrology and detailed design alternatives for the new bridge.
 - Andrew Ross provided a description of the current Teeswater River Bridge, including details on the existing deficiencies present in the structure and specialized features associated with the bridge.
 - Kelly Vader reviewed a flow chart outlining the Municipal Class EA process. She explained the various phases that were included in the EA process and that all 5 phases would be completed for a more complex project like the Teeswater River Bridge project. Kelly explained that we are currently in phase 3 and will be reviewing detailed design alternatives associated with the bridge design.
 - Kelly reviewed the consultation that has been completed as part of the EA process to date. She noted that a significant number of comments had been received from residents, particularly after signs were erected at the bridge advertising a website dedicated to the project.
 - Kelly then reviewed input received from various agencies, including Ministry of the Environment, Conservation and Parks, Saugeen Valley Conservation Authority, Ministry of Culture, Bruce County Planning Department and the Grey Bruce Health Unit.
 - Andrew Ross reviewed the bridge hydrology investigation completed as part of the Class EA. He showed photos of historic flooding in the area and described flood control measures implemented by the Conservation Authority, which includes a flood control berm that is tied in with the current bridge.
 - Andrew explained how the existing floodplain model has been updated for the current project and how the bridge design has been adapted to improve flow conditions within the Teeswater and Saugeen Rivers. He noted that final approval will be sought from the Saugeen Valley Conservation Authority before the project can be finalized.
 - Kelly described the detour alternatives that were identified for the project, including a County Road detour, two local road detours (one east and one west), and an in-town detour that would require construction of a temporary bridge over the Saugeen River.
 - Andrew then provided a more detailed description of the alternatives and the anticipated cost to implement the various options. He concluded by indicating that the in-town detour is the preliminary preferred approach being recommended as part of the project. County Council will need to confirm this approach before a detour alternative is finalized.
 - Kelly introduced the various detailed design alternatives that are being considered as part of the Class EA process. They include different railing options, sidewalk options, and design options associated with the new bridge.
 - Andrew narrated a slide show which illustrated various railing options that are available for the new bridge. He showed examples of existing railings installed on bridges in Bruce County, Lambton County, St. Marys, and Stratford, Ontario.
 - Andrew then described the design details that were investigated for the new bridge. A view of the existing bridge deck was shown as well as a proposed deck with wider sidewalks (6 feet rather than 5 feet in width) and viewing platforms on either side. He also showed images of a two span and three

span bridge design. A review of the design through the hydrology model indicates that the two span design option is suitable for the site and will not impact flood elevations in the area.

- The proposed design recommendations for the new bridge were then described.
 - Bridge Deck – 2 lanes of traffic with viewing platforms on each side
 - Bridge Spans – 2 spans with a culvert in the former south span to accommodate flows from the former Mill Race.
 - Sidewalk – 1.8m (6 ft) sidewalk on both sides of the bridge
 - Railing – Lower height solid railing with design imprint and a decorative metal railing on top.
- Andrew then showed renderings of the proposed bridge design from various view points.
- Kelly finalized the presentation by reviewing the proposed schedule for finalization of the Class EA and construction of the new bridge. She then described the next steps needed to complete the process.

6:45 p.m. – 7:15 p.m. – Questions

After concluding the presentation, questions were invited from those members of the public who had pre-registered to attend the virtual public meeting. Copies of the meeting notes and presentation material will be made available on the County of Bruce website as well as project website.

Summary of Questions and Answers

- Q. A resident asked if speeding on the County Road could be addressed as part of the new bridge design. Residents in Paisley are concerned with speeding within the community.
- A. Andrew Ross explained that the design of the bridge would not inhibit speeding. Jim Donohoe added that the bridge is not the correct means to address speeding, but the County of Bruce has other methods to address this and will document that it is a concern and look at how it can be addressed.
- Q. A resident asked if the road over the bridge would be raised as part of the project.
- A. Andrew responded by indicating that, due to the presence of existing buildings immediately north of the bridge, and the intersection to the south, the road can't be raised without impacting these adjacent uses.
- Q. Steve Hammell, Mayor of Arran-Elderslie, asked if the project team could elaborate on the proposed timing for construction of the project in 2022.
- A. Jim Donohoe responded by indicating that the current plan would be to start construction of the temporary bridge in early 2022 and have it in place by early summer. Once completed, the existing bridge would be removed and construction of the new bridge would begin. The project would be completed in the late summer/early fall of 2023.
- Q. A resident asked if there would be street lights installed on the new bridge.
- A. Andrew Ross indicated that he would look to the Municipality of Arran-Elderslie for input on the location and style of proposed street lights. Typically the design would conform with other lighting in the community.
- Q. Dave Teeple, the Fire Chief for Paisley, asked what the turning radius would be for trucks entering the south end of the detour and for the fire trucks leaving the fire hall and heading north.

-
- A. Andrew indicated that the design of the south approach road and intersection has not been finalized however we will need to consider truck turning options when it is completed. Because the fire hall is located very close to the detour route, we will have to consider how fire trucks could complete a U-Turn to access the bridge by-pass if heading north.
- Q. A resident asked again about the possibility of raising the road through the bridge site and noted that the buildings to the north all have steps up into them, so a road raise would not be a problem.
- A. Scott McLeod, Public Works Manager for Arran-Elderslie, responded that they were working with the County of Bruce on the possible reconstruction of the road section immediately north of the bridge. As part of the design, they are looking at the possibility of raising the road and sidewalk to make building entrances more accessible.
- Q. A resident asked if some components of the temporary bridge could be left in place for use by the community to enhance the Artistic Village experience, especially given the costs associated with the temporary bridge.
- A. Miguel Pelletier responded by indicating that the temporary crossing will be designed for only a few seasons of use and planning to retain portions for permanent use would likely increase the costs. Therefore, there are no plans at present to pursue retention of any components of the temporary bridge. Andrew Ross added that the bridge is only being designed for a 50 year storm event and planning to retain the abutments might impact floodplain capacity.
- Q. A resident asked if the future of the fire hall, located adjacent to the temporary detour, had been considered as part of the project.
- A. Miguel responded that the project did not consider the Fire Hall's future as a component of the project. The project only considered the possible use of the site for location of the temporary bridge crossing.
- Q. Mark Davis, Deputy Mayor of Arran-Elderslie said he was very pleased with the information presented at the meeting and asked if a letter of support from Arran-Elderslie Council would be helpful in moving the process forward.
- A. Miguel Pelletier responded that a letter of support from Council would be welcomed. He noted that a member of County Council was at the meeting and could confirm. Mayor of Arran-Elderslie, Steve Hammell indicated that a letter of support was a fantastic idea and he hoped that more letters of support would be forthcoming from the community. He said he would be advocating for the project amongst the other County Council members.
- Q. A resident reiterated the previous comment, noting they were very pleased with the proposed bridge design and, speaking on behalf of the Paisley Chamber of Commerce, would provide a letter of support for the project to the County. They also asked what would be anticipated in regards to traffic flows over the temporary bridge during construction.
- A. Andrew Ross indicated that the longer detour route on County Roads may be designated for truck traffic, however some may still choose to use the temporary bridge. A study has not been completed on how much regular traffic might be diverted from the community during bridge construction. Miguel Pelletier added that the County and Municipalities use a 511 system to provide notice of bridge closures and road construction. This is sometimes picked up by GPS systems. However, most tourists who rely on their phones or GPS systems will most likely be directed through the community to the bridge.
- Q. Dave Teeple, the Paisley Fire Chief asked what the north end of the temporary detour would look like coming off of the dyke.
- A. Andrew Ross indicated that the design was tricky but doable and that an acceptable grade can be maintained (no steeper than 5%, which also meets accessibility requirements). He said the entrance

to Ross Street may need to be closed temporarily due to the height of the road at that location, but it would only be needed during the length of the detour.

- Q. Mark Davis asked when the next public consultation would take place.
- A. Kelly Vader responded that BMROSS would be collecting public input over the next several weeks to include in a report to County Council in advance of their review of the project. She asked that anyone interested please submit their feedback on the project. In regards to additional public consultation, she noted that the next formal notification would occur when the Notice of Study Completion is published and the Environmental Study Report is made available for the mandatory 30 day review period.
- Q. A resident indicated that they were very pleased with the proposed design of the new bridge and that all of the feedback submitted by residents appears to have been acknowledged and the design of the new bridge is quite elegant.
- A. Andrew Ross thanked the resident for their feedback.

7:15 p.m. – Meeting Conclusion

The meeting was concluded at 7:15 p.m. Miguel Pelletier thanked everyone for attending before the panelists left the meeting.

Should there be any errors or omissions to these meeting notes, please notify the undersigned.

Meeting Notes Prepared by
B. M. ROSS AND ASSOCIATES LIMITED

Kelly Vader, Environmental Planner



Paisley & District Chamber of Commerce

PO 368 Paisley, ON N0G 2N0 | paisleyvillage.ca | info@paisleyvillage.ca

May 25, 2021

Re: Bruce Road 3 Teeswater River Bridge Replacement & Proposed Detours

We are writing in support of the recommendations presented by BM Ross for the “in town detour” and design for the new Teeswater River Bridge to be constructed in downtown Paisley.

The Teeswater River bridge is on a major thoroughfare and connects our downtown. As such, this project is of significant concern to our business members and community as a whole. The construction is projected to start in spring 2022 and run through summer 2023.

Like everyone, our business community has been affected by COVID-19. Many of our businesses have been forced to close by the provincial lockdowns and have sustained a significant drop in revenue and/or losses over the past 15 months. For many of our businesses, the traffic from visitors to Grey-Bruce is critical to their bottom line.

With re-opening for this summer still uncertain, our businesses cannot sustain two more summers of significantly reduced traffic that would result from an out-of-town detour. Visitors and residents alike would probably not make the detour to the other side of the village and would choose to shop outside the community.

We are further concerned with the damage it would do to our residents' wellbeing and potential safety, as the village would be effectively divided in two.

As a result, we are strongly in support of the “Class EA Detour Alternative 3 - Detour in-town” that would see a temporary bridge across the Saugeen River constructed. We also support the recommended design and feel it suits the Paisley “Artistic River Village” brand.

While we know there will still be disruption to businesses, and we look forward to working with the Municipality of Arran-Elderslie and Bruce County to develop a plan to further mitigate this, we feel the recommendations presented by BM Ross are the best detour and design options.

Finally, we also applaud the BM Ross team for their exceptional job thus far engaging and consulting with our community. We feel they have taken our feedback seriously and into consideration.

Thank you for your consideration of our business and residents' health and wellbeing in your decision.

Sincerely,

Jennifer Christie
PDCC President

May 26, 2021

To the Members of Bruce County Council:

We, the undersigned, support the recommendations presented by BM Ross for the detour and replacement bridge design for the Teeswater River Bridge in Paisley, Ontario.

Specifically, we support the recommendation for "Class EA Detour Alternative 3 - Detour in-town".

The projected timeline for the Teeswater River Bridge replacement will result in the bridge being closed to traffic from May 2022 - August 2023. During this time it is critical for the wellbeing and safety of the residents and businesses of Paisley and the surrounding community to have the ability to travel across the village.

The other detour alternatives would effectively cut the town in half, posing great health and safety risks to residents and threatening our businesses' survival.

We also support the proposed bridge design and applaud the public consultation process thus far.

1. Jennifer Christie, Paisley
2. Chris Shaw, Paisley
3. David Cormack, Paisley
4. Sarah Pedersen, Paisley
5. Sue Rath, Paisley
6. Elizabeth Carter, Paisley
7. Maxwell Johnston
8. Christine Jones
9. Rob Bonderud
10. Michael Read
11. Terryn Read
12. Sandra Crockard, Paisley
13. Alan Richardson, Paisley
14. Sibylle Walke
15. Kathy Dowe, Paisley
16. Erica MacNamara, Paisley
17. Mike Dowe, Paisley
18. Jennifer Harris, Paisley/Chesley
19. Tori Rice, Paisley
20. Loral Christie, Tara
21. John Noble
22. Nathan Braida, Paisley

23. Marian Tevelde-Sheardown, Paisley
24. Tammie McFarlane, Paisley
25. Jodi Kozelenko, Paisley
26. Ivan Steeves, Paisley
27. Dominic Tsang, Paisley
28. Jenn Budreau, Paisley
29. Scott Jennings, Paisley
30. Karen Kimpel, Paisley
31. Allison Shears, Paisley
32. Terry Haight, Paisley
33. Melissa Barrett, Dobbinton
34. Kristen Wiebe, Paisley
35. Travis Bondar, Paisley
- 36 Tom Stark Brockton
- 37 Ken Cormack
- 38 Amber Gauthier, Paisley
- 39 Joseph Leifso, Paisley
- 40 Catherine Steeves, Paisley
- 41 Shawn Norlock
42. Matthew Braid, Paisley
43. Laura Stockland, Paisley
44. Marion Lewis, Paisley
45. Julia Steeves, Paisley
46. Derek Smith, Paisley
47. Kelly McAulay, Paisley
48. Glenna Warzin, Paisley
49. Ron Warzin, Paisley
50. Susan Parker, Paisley
51. Paul Parker, Paisley
52. Beth Howe, Paisley
53. Barry Howe, Paisley
54. Lewis Coffman, Paisley



Transportation & Environmental Services Committee Minutes

June 17, 2021

Electronic (Remote) Meeting

Present	Janice Jackson, Warden Milt McIver, Councillor (Chair) Robert Buckle, Councillor Luke Charbonneau, Councillor	Gerry Glover, Councillor Steve Hammell, Councillor Chris Peabody, Councillor
Regrets	Mitch Twolan, Councillor	
Staff	Sandra Datars Bere, Chief Administrative Officer Miguel Pelletier, Director of Transportation and Environmental Services Megan Garland, Director, Long Term Care and Senior Services Christine MacDonald, Director of Human Services Cathy McGirr, Director of Museum and Cultural Services Brooke McLean, Director of Library Services	Mark Paoli, Director of Planning & Development Susan Petrik, Director of Workplace Engagement Services Steve Schaus, Director of Paramedic Services Donna Van Wyck, Clerk Darlene Batte, Deputy Clerk Adam Ferguson, Corporate Communications Specialist Jim Donohoe, Engineering Manager Kevin Predon, Forestry Technician

1. Call to Order

The meeting was called to order at 11:44 a.m.

2. Declaration of Pecuniary Interest

There were no declarations of pecuniary interest.

3. Action Items

a. Designate Japanese Knotweed and Garlic Mustard as Local Weed

Moved by Councillor Robert Buckle
Seconded by Warden Janice Jackson

That Bruce County Council designate Japanese Knotweed as a local weed through the creation of a Municipal By-Law; and,

That the Bruce County Council designate Garlic Mustard as a local weed through the creation of a Municipal By-Law.

Carried

b. A By-law to Regulate Parking, Standing, or Stopping of Vehicles on County Roads and Designated Areas within the County of Bruce Roads System

Moved by Warden Janice Jackson
Seconded by Councillor Gerry Glover

That a by-law be introduced to regulate parking, standing, or stopping of vehicles on County Roads and Designated Areas within the County of Bruce Roads System; and

That By-laws 2016-077 and 2021-017 be repealed.

Carried

c. The Teeswater River Bridge, Bruce Road 3, Paisley

Moved by Councillor Steve Hammell
Seconded by Councillor Gerry Glover

That the preferred alternative for the preliminary design (concrete bridge with two spans, two lanes, two 1.8m sidewalks) for the Teeswater River Bridge project on Bruce Road 3 in Paisley be approved; and,

That the preferred detour option (Detour 3 - temporary bridge) for the Teeswater River Bridge Replacement Project on Bruce Road 3 in Paisley be approved.

Carried

d. ATV Traffic on Bruce Road 3 in Paisley

Moved by Councillor Chris Peabody

Seconded by Councillor Steve Hammell

That All Terrain Vehicle Traffic and Multipurpose Off Highway Utility and Recreational Vehicles be permitted on Bruce Road 3 between Balaklava Street and the Bruce-Saugeen Townline in Paisley until repairs are completed on the adjacent Rail Trail; and,

That a By-law be introduced to regulate and control movement of “All-Terrain Vehicles” (ATV’s) and “Multipurpose Off Highway Utility and Recreational Vehicles” (Off-Road Vehicles or UTV) on sections of roads under the County’s jurisdiction; and,

That By-law Number 2018-034, be repealed.

Carried

4. Information Items

The following report was received for information:

- a. Capital Projects Status Report - June 2021

5. Closed Meeting

Moved by Councillor Gerry Glover

Seconded by Warden Janice Jackson

That the Committee move into a closed meeting pursuant to:

Section (2) (c) - a proposed or pending acquisition or disposition of land by the municipality or local board; and,

Section 239 (2) (f) - advice that is subject to solicitor-client privilege, including communications necessary for that purpose;

to discuss:

- a. Bruce Road 1 Realignment Property Transfer - Russells
- b. Approval of Closed Minutes - March 18, 2021

Carried

6. Reporting from Closed

The Chair reported that direction was provided to staff and the County Solicitor related to Bruce Road 1 Realignment Property Transfer - Russells and the closed minutes of the March 18, 2021 meeting were approved.

7. Act on Recommendations

Moved by Warden Janice Jackson

Seconded by Councillor Gerry Glover

That in accordance with the Procedure By-law, staff be authorized and directed to give effect to the actions of the Transportation & Environmental Services Committee in respect of all resolutions passed during the June 17, 2021 meeting.

Carried

8. Next Meeting

The next meeting of the Transportation and Environmental Services Committee will take place electronically on July 8, 2021.

9. Adjournment

Moved by Councillor Gerry Glover

That the meeting of the Transportation and Environmental Services Committee adjourn at 12:40 p.m.

Councillor Milt McIver, Chair

Transportation & Environmental Services Committee

B. M. ROSS AND ASSOCIATES LIMITED
Engineers and Planners
62 North Street, Goderich, ON N7A 2T4
p. (519) 524-2641 • f. (519) 524-4403
www.bmross.net

File No. 13127

July 28, 2021

Saugeen Ojibway Nation (SON) – Chippewas of Saugeen &
Chippewas of Nawash
Emily Martin
25 Maadookii Subdivision
Neyaashiinigmiing, ON N0H 2T0

**RE: Class EA to Replace the Teeswater River Bridge
County of Bruce (Paisley)**

The County of Bruce initiated a Class Environmental Assessment (Class EA) process in 2019 to consider options associated with replacement of the Teeswater River Bridge which spans the Teeswater River along Bruce Road 3 in Paisley, immediately north of the intersection with Bruce Road 11 (as shown on the accompanying key plan). We have forwarded project information previously for this project, but are unsure, given restrictions related to the pandemic, whether SON has had an opportunity to review the information.

The Class Environmental Assessment process is now nearing completion. As this area forms part of the traditional territory of the Saugeen Ojibway Nation, we are seeking your input on the preferred alternative and proposed detour route prior to finalization. The preferred alternative selected for this project includes construction of a two-span bridge to replace the existing crossing. The new bridge would be constructed in generally the same location as the existing bridge and contain wider sidewalks, viewing platforms, and a decorative metal railing (see attached conceptual renderings).

Construction of the new bridge will take 14-16 months requiring a detour for vehicular and pedestrian traffic. The preferred detour route selected would direct local and through traffic over a temporary bridge, which would be constructed over the Saugeen River, between Church Street and Goldie Street (see attached plan). Construction of the temporary bridge will not encroach within the Saugeen River, however a temporary road access will be constructed immediately west of the Fire Hall off of Bruce Road 11 (Goldie Street)

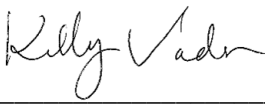
A number of mitigation measures will be completed to avoid or mitigate impacts to aquatic species and their habitats, including mussel searches, fish salvages, and adherence to in-water timing windows. A Stage 1 & 2 Archaeological Assessment will also be completed for areas that would be disturbed during construction of the temporary detour bridge. SON will be contacted by Timmins Martelle Heritage Consultants (TMHC) in advance of the field work to seek your input.

We have enclosed preliminary engineering drawings of the new bridge, a map showing the route of the proposed temporary detour, and renderings of the proposed replacement crossing.

Please let us know if you have any comments or questions on the proposed project. We look forward to hearing from you and ask that you send a response to our office by September 10, 2021. If you have any questions or require further information, please contact the undersigned at 519-524-2641 or by e-mail at kvader@bmross.net.

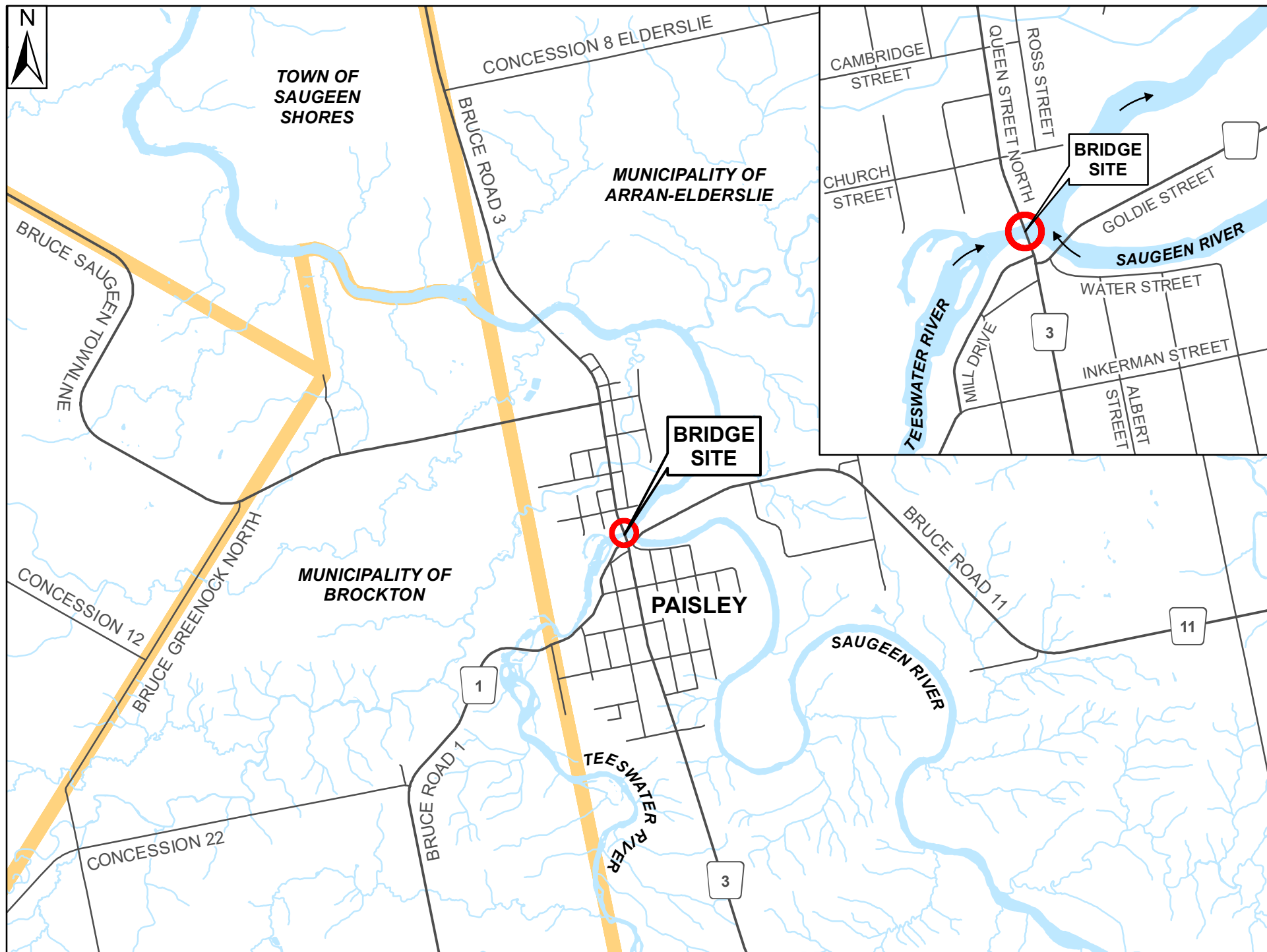
Yours very truly

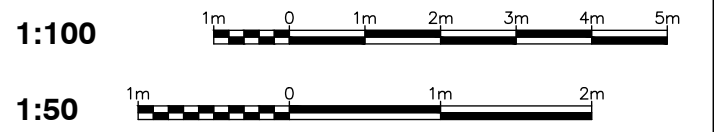
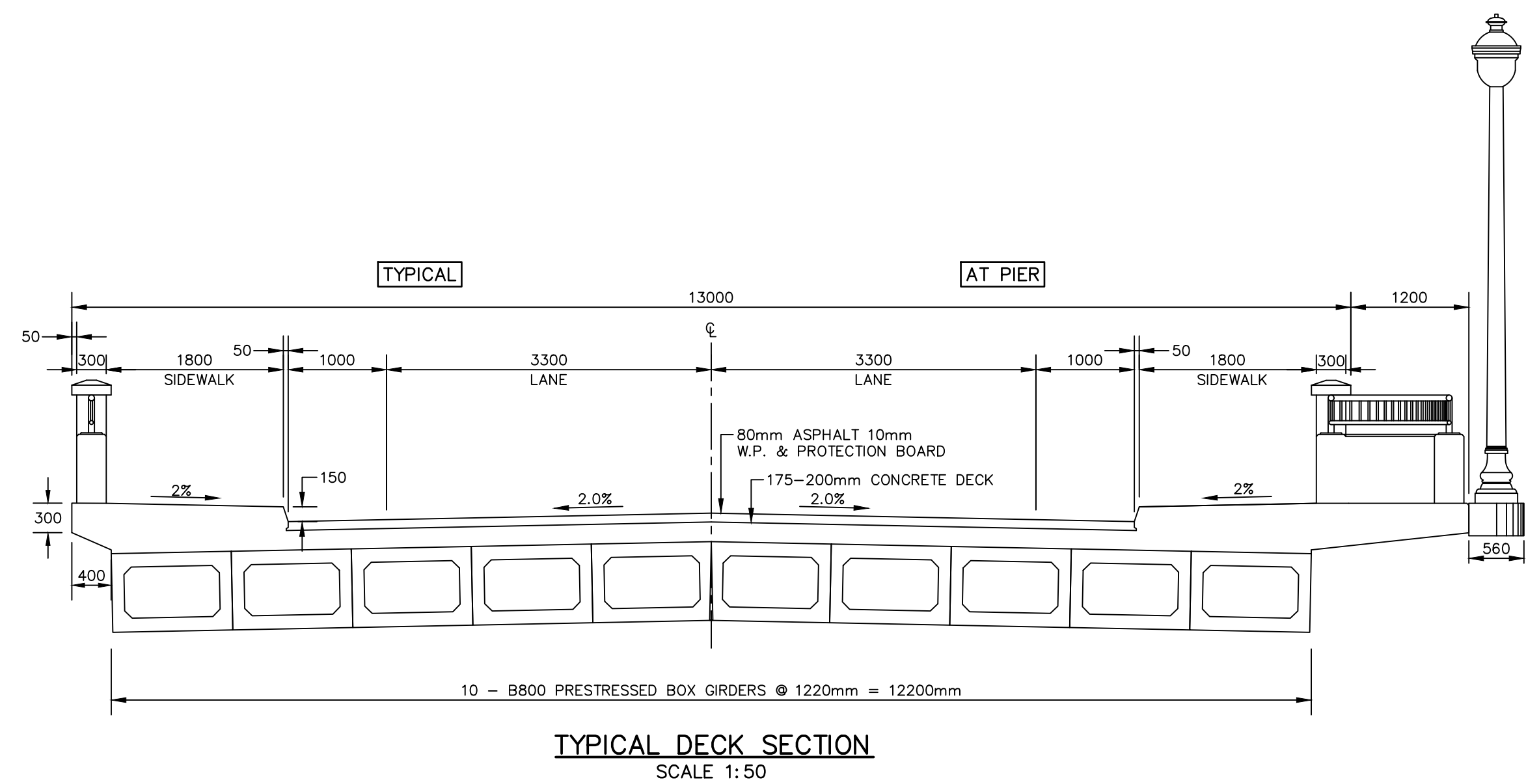
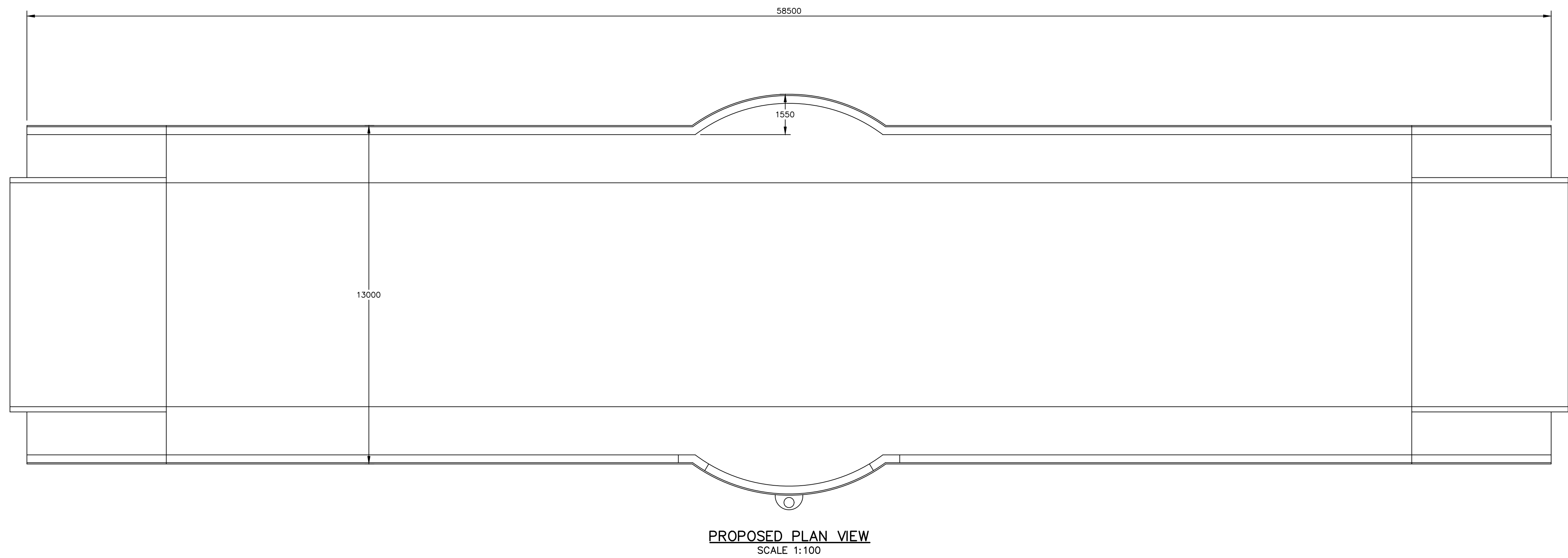
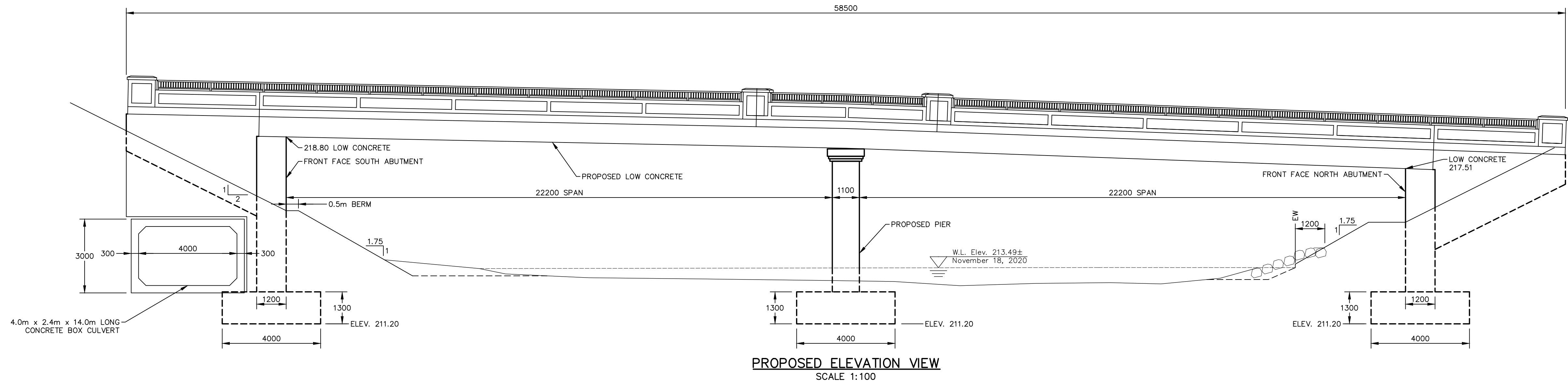
B. M. ROSS AND ASSOCIATES LIMITED

Per 
Kelly Vader, MCIP, RPP
Environmental Planner

KV:jf
Encl.

cc. Jim Donohoe, County of Bruce
Juanita Meekins, Saugeen Ojibway Nation





Design By: M.A.B. Checked By: A.I.R.

PRELIMINARY

No.	DATE	REVISION
1.	May 7, 2021	Issued for Pricing
2.	July 18, 2021	Issue for County Review

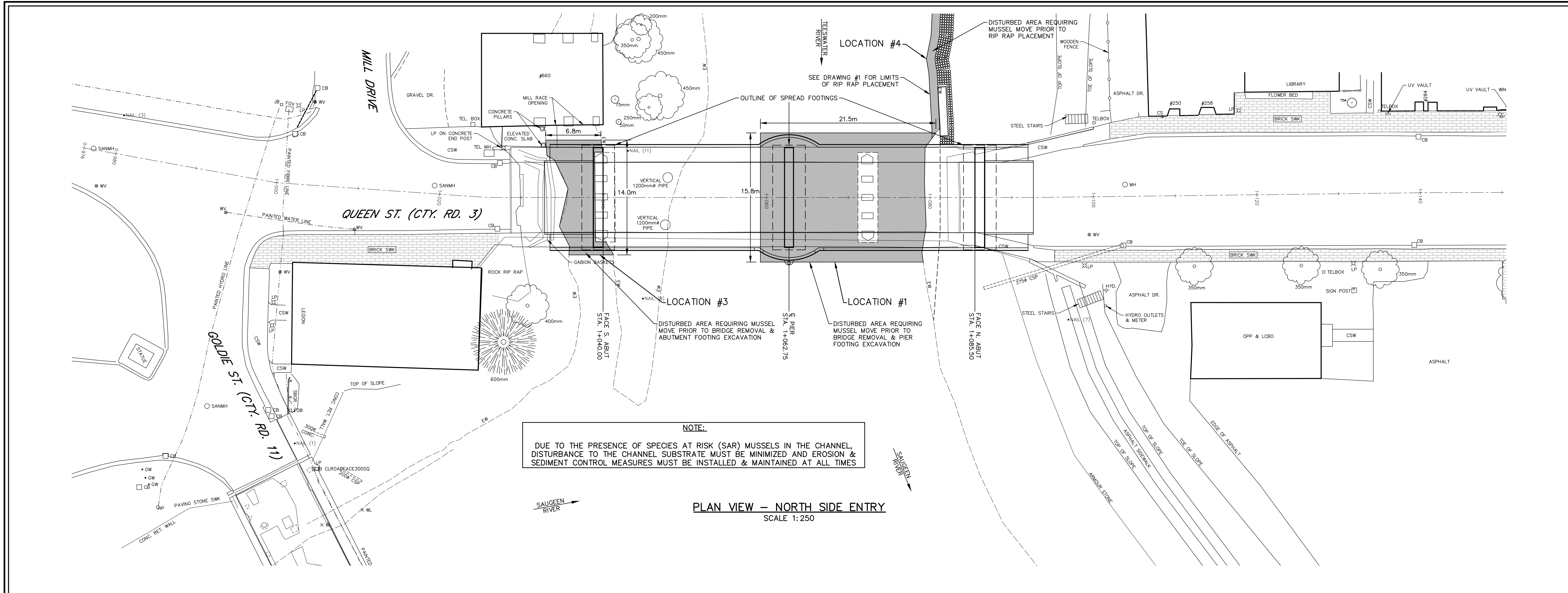


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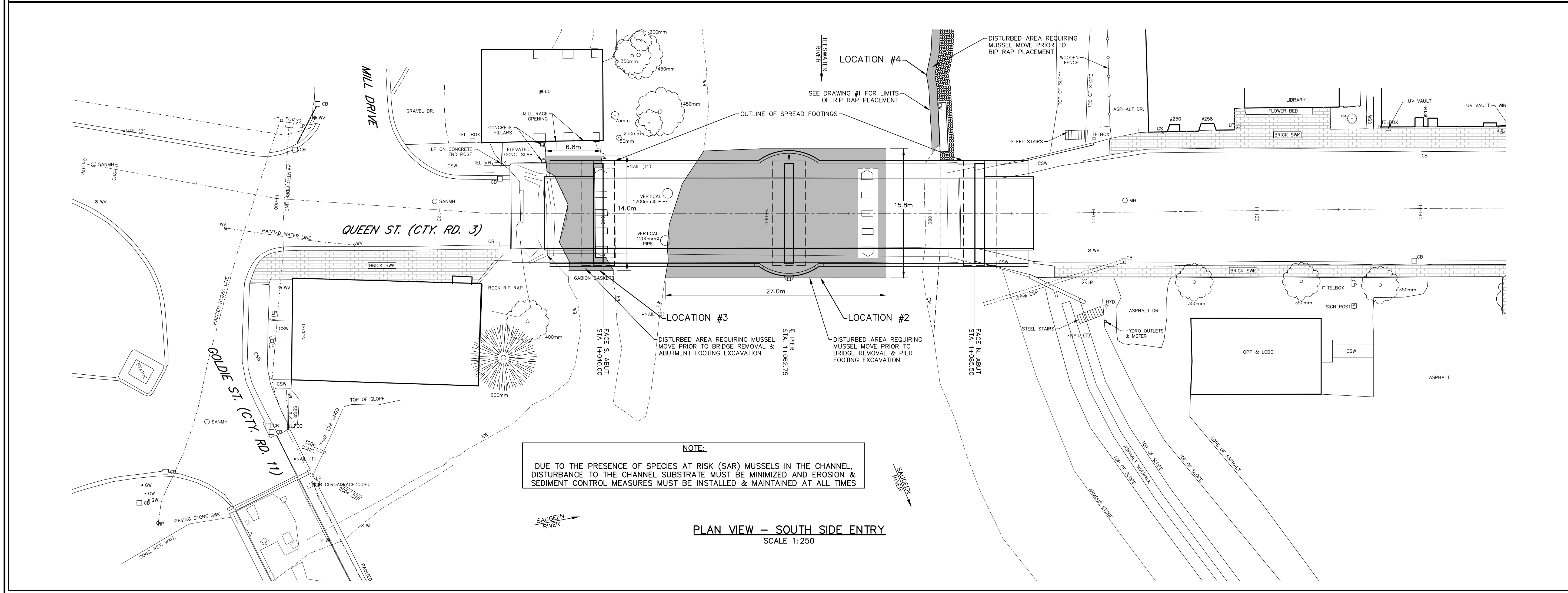


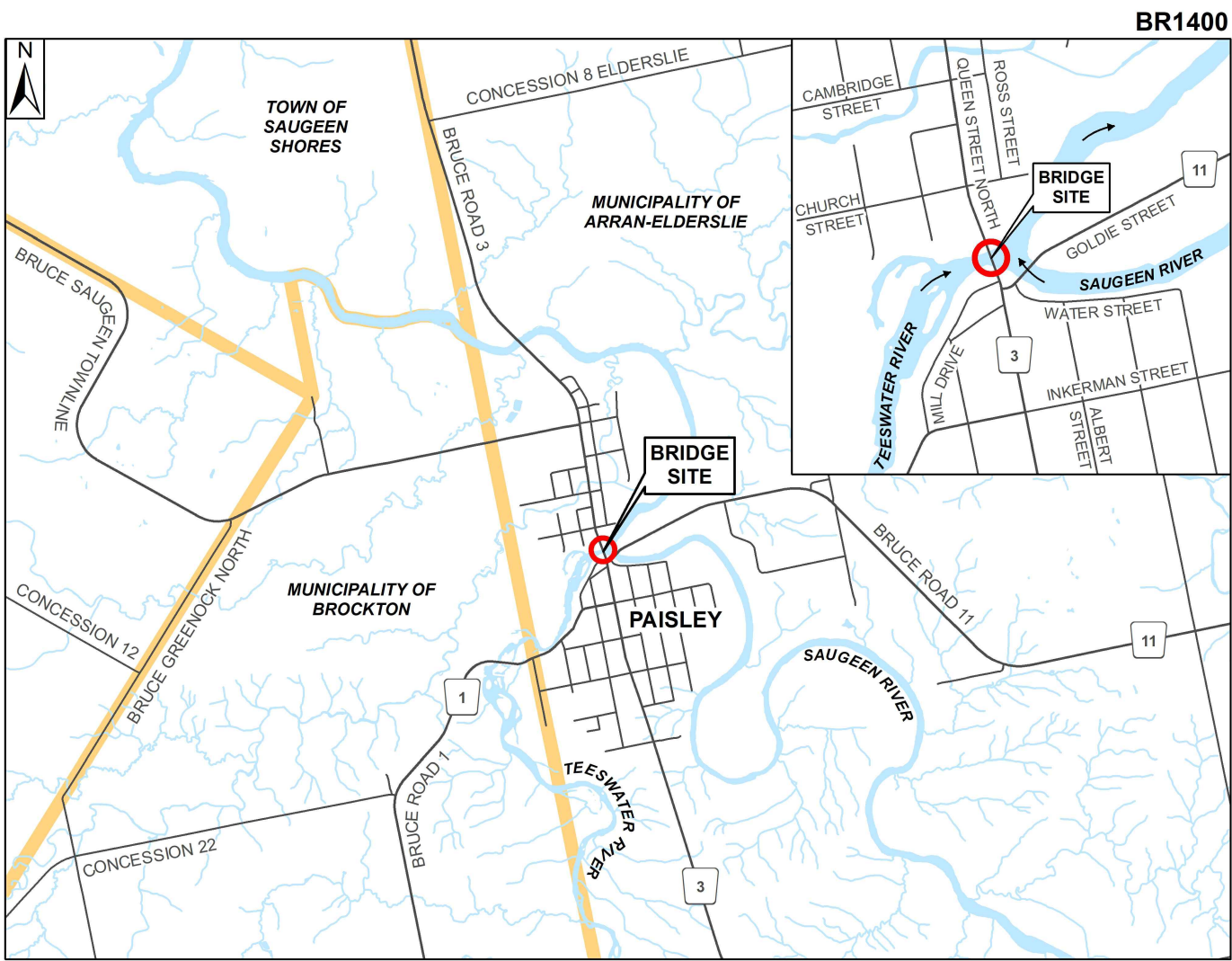
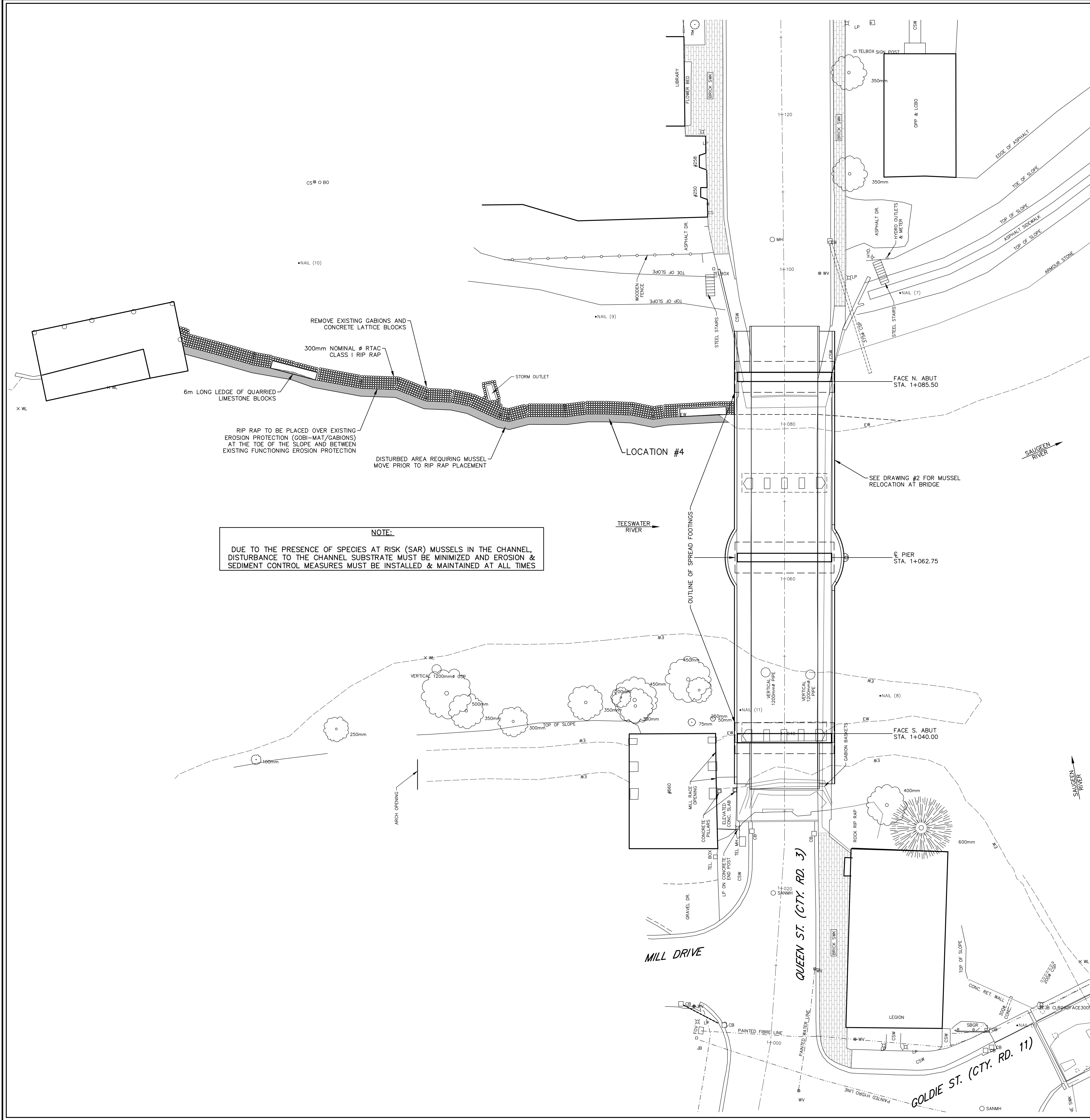
County of Bruce
Teeswater River Bridge
Two Span Bridge with Lookouts
Elevation, Plan View & Deck Section

Structure No. 0333300	Project No. BR1400
Scale (24x36) As Shown	Drawing No. 3 of 4

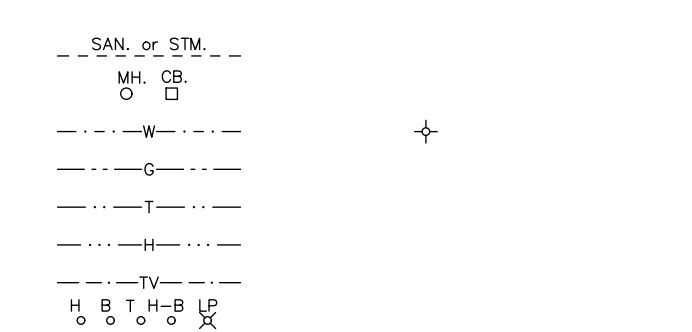


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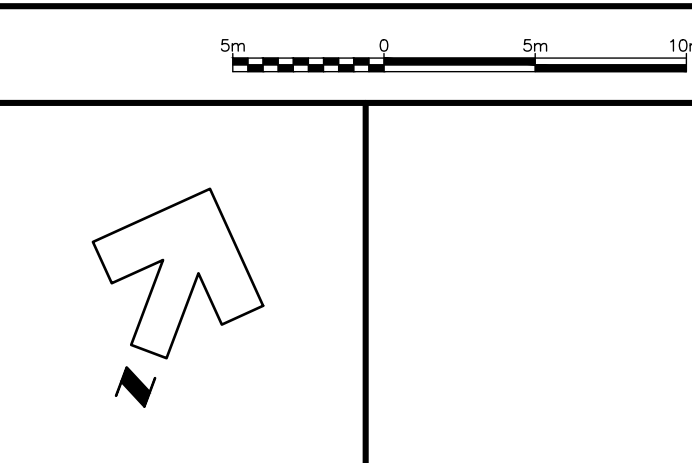




KEY PLAN
NOT TO SCALE



MUSSEL REMOVAL AREA	
Location	Area (m ²)
#1 - North Side Entry	292.0
#2 - South Side Entry	376.0
#3 - Mill Race	63.0
#4 - Along Riverbank	71.0
North Side Total (#1+#3+#4)	426.0
South Side Total (#2+#3+#4)	510.0



B.M. Elev. 221.107
Nail & flag in North face of HP, East of Goldie St.
bridge on the South side of the road
Northing: 4905973.431
Easting: 0478329.827

Design By: M.A.B. Checked By: K.V.

PRELIMINARY

No.	DATE	REVISION
1.	July 9, 2021	Issued for Review by D.F.O.
2.	July 19, 2021	Issue for County Review



Goderich Mount Forest Sarnia

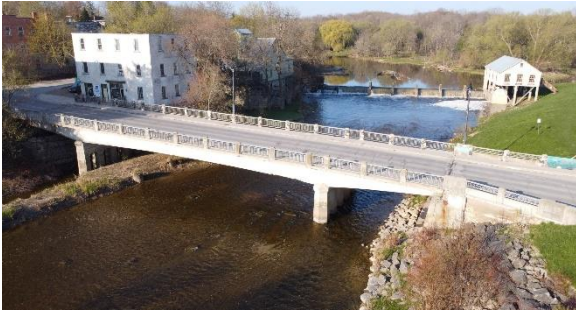


**County of
Bruce**
Teeswater River Bridge
Plan View

Structure No. 0333300	Project No. BR1400
Scale (24x36) 1:250	Drawing No. 1 of 2

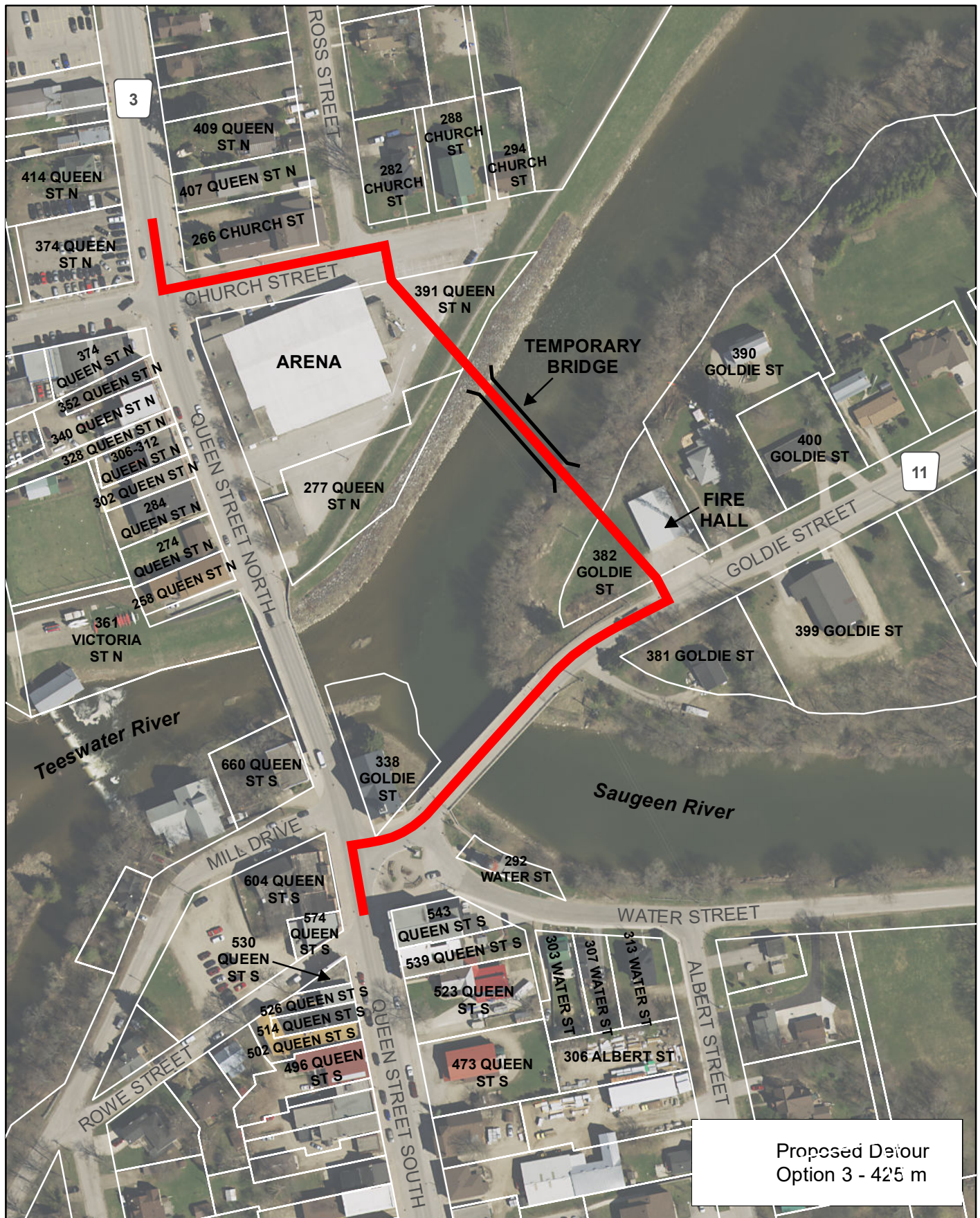
Proposed Teeswater Bridge in Paisley, Ontario

Existing



Proposed





Proposed Detour
Option 3 - 425 m



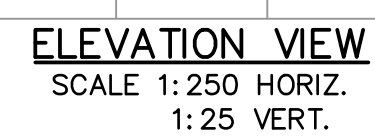
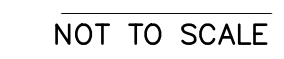
COUNTY OF BRUCE
CLASS EA FOR THE
TEESWATER RIVER BRIDGE
DETOUR OPTION 3

DATE
NOV. 11, 2019

PROJECT No.
13127

SCALE
1:2,000

FIGURE No.



1.





CLASS EA FOR THE TEESWATER RIVER BRIDGE

MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT

COUNTY OF BRUCE

NOTICE OF STUDY COMPLETION

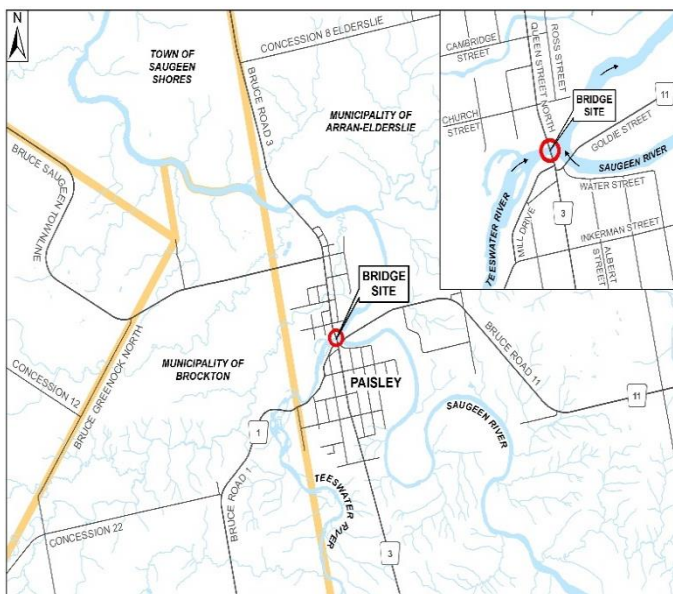
The County of Bruce initiated a Class Environmental Assessment (Class EA) process in October 2019 to consider options associated with replacement of the Teeswater River Bridge which spans the Teeswater River along Bruce Road 3 in the community of Paisley (as shown on the accompanying key plan). As part of the Class EA process, the County is considering a range of design alternatives associated with the new bridge as well as detour alternatives to allow traffic to detour around the site during construction of the new crossing. A preferred alternative for the bridge design and detour has now been selected. The proposed bridge will be a two-span structure with wider sidewalks and viewing platforms on both sides. The railing will also be designed with a lower parapet wall and decorative railing to allow unobstructed views of the river. An in-town temporary detour route was selected for local traffic to minimize impacts to residents and the commercial district during reconstruction of the bridge.

THE ENVIRONMENTAL ASSESSMENT PROCESS:

The planning for this project is following the planning process established for Schedule 'C' activities under the Municipal Class Environmental Assessment (Class EA) document. Schedule 'C' projects must complete all five phases of the Class EA, which is undertaken in order to identify potential environmental impacts associated with the proposal and to plan for appropriate mitigation of any impacts. The purpose of the Environmental Assessment process is to identify any potential environmental impacts associated with the proposal and to plan for appropriate mitigation of any impacts. The environmental assessment process has now been completed. There were no negative impacts identified with the project that could not be mitigated.

PUBLIC INVOLVEMENT:

For further information on this project, please contact the project engineers: B.M. Ross and Associates Ltd.: 62 North Street, Goderich, Ontario, N7A 2T4. Telephone (Toll Free): (888) 524-2641. Kelly Vader, Environmental Planner (e-mail: kvader@bmross.net), prior to February 6, 2022. Information will be collected in accordance with the Municipal Freedom of Information and Protection of Privacy Act. With the exception of personal information, all comments will become part of the public record. An Environmental Screening Report, documenting the environmental assessment conducted for this project, will be available for public review on the County of Bruce website at www.brucecounty.on.ca as of January 7, 2022.



Interested persons may provide written comments to the project team by February 6, 2022. All comments and concerns should be sent directly to Jim Donohoe, Engineering Manager at the County of Bruce, email: jdonohoe@brucecounty.on.ca or telephone: (519) 881-2400. In addition, a request may be made to the Ministry of the Environment, Conservation and Parks for an order requiring a higher level of study (i.e., requiring an individual/comprehensive EA approval before being able to proceed), or that conditions be imposed (e.g., require further studies), only on the grounds that the requested order may prevent, mitigate, or remedy adverse impacts on constitutionally protected Aboriginal and treaty rights. Requests on other grounds will not be considered. Requests should include the requester contact information and full name for the ministry.

Requests should specify what kind of order is being requested (request for additional conditions or a request for an individual/comprehensive environmental assessment), how an order may prevent, mitigate, or remedy those potential adverse impacts, and any information in support of the statements in the request. This will ensure that the ministry is able to efficiently begin reviewing the request. The request should be sent in writing or by email to:

Minister of the Environment, Conservation and Parks
Ministry of Environment, Conservation and Parks
777 Bay Street, 5th Floor
Toronto ON M7A 2J3
minister.mecp@ontario.ca

Director, Environmental Assessment Branch
Ministry of Environment, Conservation & Parks
135 St. Clair Ave. W, 1st Floor
Toronto ON, M4V 1P5
EABDirector@ontario.ca

Requests should also be sent to the County of Bruce by mail or by e-mail.

This Notice Issued January 7, 2022
County of Bruce

Jim Donohoe, P. Eng., Engineering Manager

**Ministry of Heritage, Sport,
Tourism and Culture Industries**

Programs and Services Branch
400 University Ave, 5th Flr
Toronto, ON M7A 2R9
Tel: 613.242.3743

**Ministère des Industries du Patrimoine,
du Sport, du Tourisme et de la Culture**

Direction des programmes et des services
400, av. University, 5e étage
Toronto, ON M7A 2R9
Tél: 613.242.3743



February 3, 2022

EMAIL ONLY

Jim Donohoe, P. Eng.
Engineering Manager
County of Bruce
jdonohoe@brucecounty.on.ca

MHSTCI File : 0011648
Proponent : County of Bruce
Subject : Notice of Study Completion – MCEA Schedule C
Project : Teeswater River Bridge
Location : Bruce Road 3 in the community of Paisley, Township of Arran-Elderslie, County of Bruce

Dear Jim Donohoe;

Thank you for providing the Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI) with the Notice of Study Completion for the above-referenced project. MHSTCI's interest in this Environmental Assessment (EA) project relates to its mandate of conserving Ontario's cultural heritage.

Under the EA process, the proponent is required to determine a project's potential impact on cultural heritage resources.

Project Summary

The County of Bruce has initiated a Class EA process to consider options associated with the replacement of the Teeswater River Bridge where Bruce Road 3 (Queen Street South) spans the Teeswater River. This project is following the planning process established for Schedule C activities as described in the Municipal Engineers Association's Municipal Class Environmental Assessment (Class EA) document.

Comments on the Project File Report

We have reviewed the above referenced notice and the Project File Report (dated January 5, 2022, prepared by B.M. Ross and Associates Ltd.) and have attached a table with detailed comments and recommendations to support documentation around cultural heritage due diligence.

Thank you for consulting MHSTCI on this project. If you have any questions or require clarification, do not hesitate to contact me.

Sincerely,

Joseph Harvey
Heritage Planner
Heritage Planning Unit
joseph.harvey@Ontario.ca

Copied to: Kelly Vader, Environmental Planner, B.M. Ross and Associates Ltd.
Karla Barboza, (A) Team Lead – Heritage, MHSTCI
Mark Badali, Environmental Resource Planner & EA Coordinator, MECP

It is the sole responsibility of proponents to ensure that any information and documentation submitted as part of their EA report or file is accurate. MHSTCI makes no representation or warranty as to the completeness, accuracy or quality of the any checklists, reports or supporting documentation submitted as part of the EA process, and in no way shall MHSTCI be liable for any harm, damages, costs, expenses, losses, claims or actions that may result if any checklists, reports or supporting documents are discovered to be inaccurate, incomplete, misleading or fraudulent.

Please notify MHSTCI (at archaeology@ontario.ca) if archaeological resources are impacted by EA project work. All activities impacting archaeological resources must cease immediately, and a licensed archaeologist is required to carry out an archaeological assessment in accordance with the Ontario Heritage Act and the Standards and Guidelines for Consultant Archaeologists.

If human remains are encountered, all activities must cease immediately, and the local police and coroner must be contacted. In situations where human remains are associated with archaeological resources, MHSTCI should also be notified (at archaeology@ontario.ca) to ensure that the site is not subject to unlicensed alterations which would be a contravention of the Ontario Heritage Act.

Item	Document Section	Given Text	MHSTCI Comments
1.	2.7) Cultural Environment Page 24	Based on input received from the Ministry of Tourism, Culture, and Sport (MTCS), ...	Please also ensure that the correct name of our ministry is used. "Ministry of Tourism, Culture, and Sport (MTCS)" should be replaced with "Ministry of Heritage, Sport, Tourism and Culture Industries"
2.	2.7.1) Archaeological Resources Page 24	Certain areas of the site and study area have high archaeological potential (e.g., areas within 300m of a watercourse). For this reason, a stage 1 & 2 Archaeological Assessment may be needed for the project if the recommended improvements involve disturbance of native soils.	<p>Section 5.3 Indicates that the Municipal Heritage Bridges: Cultural, Heritage & Archaeological Resources Assessment Checklist was completed determining there was potential for archaeological resources within the study area. It is further indicated that a Stage 1 & 2 Archeological Assessment has been completed for this undertaking.</p> <p>We recommend that section 2.7.1 be revised and suggest the following text:</p> <p>The Municipal Heritage Bridges Cultural, Heritage and Archaeological Resources Assessment Checklist, developed by the Municipal Engineers Association, was completed and included in appendix G. Part D of the checklist indicates that an archaeological assessment will be required for the study area.</p> <p>A combined Stage 1-2 archaeological assessment (AA) (under Project Information Form number P1078-0146-2021) was undertaken on November 16, 2021 by Timmins Martelle Heritage Consultants Inc. in support of this environmental assessment. The Stage 1-2 AA has been entered into the Ontario Public Register of Archaeological Reports and is included in Appendix G.</p> <p>[Then include the outcomes and recommendations of the report, as is in the Executive Summary – just copy and paste, don't summarize]</p>

Item	Document Section	Given Text	MHSTCI Comments
3.	2.7.2) Built Heritage Resources (Paragraph 2) Page 24	... The report found that the structure has cultural heritage and for this reason, a Heritage Impact Assessment (HIA) was also prepared ...	Terminology should be consistent with the Ontario Heritage Act. We recommend that the text be revised as follows: The report found that the structure has cultural heritage <u>value or interest</u> and for this reason, a Heritage Impact Assessment (HIA) was also prepared ...
4.	Table 3.1: Evaluation of Alternatives: Identification of Environmental Components Page 29	Element <ul style="list-style-type: none"> • Cultural Component <ul style="list-style-type: none"> • Heritage • Archaeology Sub-Component <ul style="list-style-type: none"> • Historical/ Cultural Resources • Buried Historical Artifacts 	We recommend the following revision to better align with the current terminology and Section 2.7 of the Screening Report. Element <ul style="list-style-type: none"> • Cultural Heritage <u>Environment</u> Component <ul style="list-style-type: none"> • <u>Built Heritage Resources and Cultural Heritage Landscapes</u> • Archaeological <u>Resources</u> Sub-Component <ul style="list-style-type: none"> • Historical/ Cultural Resources <u>Identified heritage attributes of the bridge</u> • Buried Historical Artifacts <u>Archaeological resources and areas of archaeological potential</u>
5.	Table 3.3: Environmental Effects Analysis Page 35	Environmental Component (Subcomponent) <ul style="list-style-type: none"> • Cultural (Built Heritage) • Cultural (Archaeology) 	See comment above. We recommend the following revision: Environmental Component (Subcomponent) <ul style="list-style-type: none"> • Cultural Heritage (Built Heritage <u>Resources and Cultural Heritage Landscapes</u>) • Cultural <u>Heritage</u> (Archaeology <u>Resources</u>)

Item	Document Section	Given Text	MHSTCI Comments
6.	7.7) Summary of Impact Mitigation Page 108	Missing information	This section should indicate that the recommendations of the Cultural Heritage Evaluation Report & Heritage Impact Assessment: Bruce Road 3 Bridge (Queen Street Bridge) Paisley, Arran-Elderslie, Bruce County, ON (dated October 2021, prepared by Timmins Martelle Heritage Consultants Inc.) will be adhered to.
7.	8.0) Approvals Page 109	Missing sub-section	<p>We recommend including an additional subsection addressing the <i>Ontario Heritage Act</i>. Please see the following text</p> <p>8.5 Ontario Heritage Act</p> <p>If archaeological resources are impacted by EA project work, please notify MHSTCI at archaeology@ontario.ca. All activities impacting archaeological resources must cease immediately, and a licensed archaeologist will carry out an archaeological assessment in accordance with the Ontario Heritage Act and the Standards and Guidelines for Consultant Archaeologists.</p> <p>If human remains are encountered, all activities must cease immediately and the local police and coroner. In situations where human remains are associated with archaeological resources, MHSTCI should also be notified to ensure that the site is not subject to unlicensed alterations which would be a contravention of the Ontario Heritage Act.</p>

**Ministry of the Environment,
Conservation and Parks**

**Ministère de l'Environnement,
de la Protection de la nature
et des Parcs**

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February 6, 2022

Jim Donohoe
Engineering Manager
County of Bruce
jdonohoe@brucecounty.on.ca

**Re: Teeswater River Bridge (Paisley)
County of Bruce
Municipal Class Environmental Assessment – Schedule C
Project Review Unit Comments – Environmental Study Report**

Dear Jim Donohoe,

This letter is in response to the Notice of Completion provided for the Environmental Study Report (ESR) for the above noted Class Environmental Assessment (EA) project. Our understanding is that in order to resolve significant deficiencies with structural components of the Teeswater River Bridge on Bruce Road 3 in central Paisley, Bruce County (the proponent) has determined that the preferred alternative is to replace the existing three-span water crossing with a two-span bridge in the same location with an embossed lower parapet wall, a wider deck and two viewing areas, using an in-town temporary detour. The Ministry of the Environment, Conservation and Parks (ministry) provides the following comments for your consideration.

Evaluation of Alternatives

- 1) Table 3.3 in Section 3.5 of the ESR presents on environmental effects analysis by identifying the Level of Effect on Environmental Components for each alternative. The results of this analysis are summarized in Table 3.4 on Page 40. There are several inconsistencies between Table 3.3 and Table 3.4, which if intentional require explanation, or if not require revision so that the Level of Effects are consistent across both tables:

- a. For Environmental Component “***Natural (Aquatic)***”, **Alternative 3**:
 - Table 3.3 shows Level of Effect as **Low-Moderate**.
 - Table 3.4 shows Level of Effect as **Moderate**.
- b. For Environmental Component “***Natural (Hydrogeologic)***”, **Alternative 2**:
 - Table 3.3 shows Level of Effect as **Low-Moderate**.
 - Table 3.4 shows Level of Effect as **Moderate**.
- c. For Environmental Component “***Natural (Hydrogeologic)***”, **Alternative 3**:
 - Table 3.3 shows Level of Effect as **Low**.
 - Table 3.4 shows Level of Effect as **Moderate**.
- d. For Environmental Component “***Social (Neighbourhood)***”, **Alternative 1**:
 - Table 3.3 shows Level of Effect as **Low-Moderate**.
 - Table 3.4 shows Level of Effect as **Moderate**.
- e. For Environmental Component “***Social (Neighbourhood)***”, **Alternative 2**:
 - Table 3.3 shows Level of Effect as **Moderate**.
 - Table 3.4 shows Level of Effect as **Moderate-High**.
- f. In Table 3.4, the **titles** for the Environmental Subcomponents “***Archaeological Resources***” and “***Built Heritage Resources***” should be switched.
- g. For Environmental Component “***Economic (Project Area)***”, **Alternative 1**:
 - Table 3.3 shows Level of Effect as **Moderate**.
 - Table 3.4 shows Level of Effect as **Moderate-High**.
- h. For Environmental Component “***Economic (Project Area)***”, **Alternative 2**:
 - Table 3.3 shows Level of Effect as **Moderate**.
 - Table 3.4 shows Level of Effect as **Moderate-High**.
- i. For Environmental Component “***Economic (Project Area)***”, **Alternative 3**:
 - Table 3.3 shows Level of Effect as **Low-Moderate**.
 - Table 3.4 shows Level of Effect as **Low**.

Indigenous Consultation

- 2) As previously noted in the ministry’s Project Review Unit (PRU) Comments letter dated December 3, 2021 provided in response to the draft Report, please ensure that the Notice of Study Completion has been provided to potentially impacted communities. The record of consultation should be updated with this circulation and with any follow-up correspondence. Further to any follow-up during the review period for the EA, the proponent should continue reaching out to all communities previously engaged if there any substantial changes to the

project/process or if they are applying for subsequent permits from the ministry that may be of interest or concern to communities.

Excess Materials and Waste

- 3) As previously noted in the ministry's PRU Comments letter dated December 3, 2021, please note that for any excess material that is excavated and requires disposal and sampling, the Surface Water Unit of the ministry's Technical Support Section can provide review and advice to ensure concurrence with O. Reg. 406/19.

Noise and Vibration

- 4) Section 6.2.3 of the ESR states, "*Properties located in close proximity to the bridge site could experience some direct impacts from construction (e.g. noise...*" Further, Section 7.7 of the ESR states, "*Construction activities will be conducted in accordance with... impact mitigation requirements of various regulatory agencies.*" As previously noted in the ministry's PRU Comments letter dated December 3, 2021, noise control measures should be incorporated into construction mitigation plans in order to mitigate adverse noise impacts to nearby residential land uses within the study area during construction activities. The ESR should be revised to state what noise control measures will be implemented.

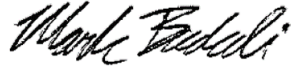
Surface Water

- 5) As previously noted in the ministry's PRU Comments letter dated December 3, 2021, further to the discussion on dewatering in Section 6.2.2 of the ESR, please note that a sampling and monitoring plan is required to ensure that turbid water pumped from the excavation is properly treated in order to protect the receiver. Where a permit to take water is required, the proponent can work with the Groundwater Unit and Surface Water Unit of the ministry's Technical Support Section in the development of this plan.
- 6) As previously noted in the ministry's PRU Comments letter dated December 3, 2021, further to the discussions on erosion and sediment control measures in Sections 6.2.2 and 7.7 of the ESR, an erosion and sediment control plan will need to be developed, as sediment and erosion control measures during construction is critical both in terms of protecting the water quality and reducing the impacts to local aquatic community. The proponent can work with the Surface Water Unit of the ministry's Technical Support Section and the Saugeen Valley Conservation Authority to develop a robust monitoring and mitigation plan that will augment the overarching erosion and sediment control plan.

Thank you for circulating this Report for the ministry's consideration. Please document the receipt of this Project Review Unit Comments letter in the final report. We look forward to receiving a written response from the County of Bruce to address our comments provided above.

Should you or any members of your project team have any questions regarding the material above, please contact me at mark.badali1@ontario.ca.

Sincerely,

A handwritten signature in black ink, reading "Mark Badali". The signature is written in a cursive, flowing style.

Mark Badali
Regional Environmental Planner
Project Review Unit, Environmental Assessment Branch
Ontario Ministry of the Environment, Conservation and Parks

cc John Ritchie, Manager, Owen Sound District Office, MECP
Kelly Vader, Environmental Planner, B. M. Ross and Associates Limited